

Certificate of Approval

AMENDMENT NO. 148

TO THE TOWN OF GEORGINA OFFICIAL PLAN

(KESWICK SECONDARY PLAN)

This Secondary Plan document which was adopted by the Council of the Corporation of the Town of Georgina is approved pursuant to Sections 17 and 21 of the Planning Act and came into force on _____, 2022.

Date: _____

**Karen Whitney, MCIP, RPP
Director of Community Planning and
Development Services
The Regional Municipality of York**

TABLE OF CONTENTS

AMENDMENT NO. 148

TO THE TOWN OF GEORGINA OFFICIAL PLAN

(KESWICK SECONDARY PLAN)

PART A – THE CERTIFICATION	PAGES
1. The Certification Page	1
2. By-law No. 2022 - 0088 (PL-2) adopting Amendment No. 148	2
PART B – THE PREAMBLE	
1. Title	3
2. Components of the Amendment	3
3. Purpose	3
4. Location	3
5. Basis	3 and 4
PART C – THE AMENDMENT	
1. Introduction	5
2. Actual Amendment	5
3. Implementation	5
4. Interpretation	6
SCHEDULE ‘A’ – LOCATION MAP	
ATTACHMENT ‘A’ – THE KESWICK SECONDARY PLAN	

PART A – THE CERTIFICATION PAGE

AMENDMENT NO. 148

TO THE TOWN OF GEORGINA OFFICIAL PLAN

(KESWICK SECONDARY PLAN)

The attached explanatory text and location map, constituting Amendment No. 148 to the Town of Georgina Official Plan (Keswick Secondary Plan), was adopted by the Council of the Corporation of the Town of Georgina by By-law No. 2022 - 0088_ (PL-2) pursuant to Sections 17 and 21 of the Planning Act, R.S.O. 1990, on the 7th day of September, 2022.

Margaret Quirk, Mayor

Mamata Baykar, Deputy Clerk

THE CORPORATION OF THE TOWN OF GEORGINA

IN THE

REGIONAL MUNICIPALITY OF YORK

BY-LAW NUMBER 2022- 0088 (PL-2)

**BEING A BY-LAW TO ADOPT AMENDMENT NO. 148 TO THE TOWN
OF GEORGINA OFFICIAL PLAN (KESWICK SECONDARY PLAN)**

The Council of the Corporation of the Town of Georgina, pursuant to Sections 17 and 21 of the Planning Act, R.S.O. 1990, c.P.13, as amended, hereby **ENACTS AS FOLLOWS:**

1. **THAT** Amendment No. 148 to the Town of Georgina Official Plan (Keswick Secondary Plan) constituting the attached explanatory text, is hereby adopted.
2. **THAT** the Corporation of the Town of Georgina make application to York Region for approval of said Amendment.
3. **THAT** the Clerk of the Corporation of the Town of Georgina is hereby authorized and directed to make such application on behalf of the Corporation and to execute under the Corporate Seal such documents as may be required for the above purposes.

READ and ENACTED this 7th day of September, 2022.

Margaret Quirk, Mayor

Mamata Baykar, Deputy Clerk

PART B – THE PREAMBLE

1. TITLE

This Amendment shall be known as:

Amendment No. 148
to the Town of Georgina Official Plan
(Keswick Secondary Plan)

2. COMPONENTS OF THE AMENDMENT

Only that part of this document entitled "Part C – The Amendment", comprising the attached explanatory text, constitutes Amendment No. 148 to the Town of Georgina Official Plan (Keswick Secondary Plan).

3. PURPOSE

The purpose of Amendment No. 148 is to delete the existing "Keswick Secondary Plan", as amended, which forms Section 13.1 of the Town of Georgina Official Plan and replace it with a new Section 13.1, which constitutes a new "Keswick Secondary Plan".

4. LOCATION

Amendment No. 148 applies to the lands shown on Schedule 'A' – Location Map, to this Amendment.

5. BASIS

The Keswick Secondary Plan (KSP) forms an integral part of the Official Plan, and provides a more detailed vision and land use policies for the community of Keswick. The current KSP was approved in 2004 and, since its approval, has been amended numerous times mainly to accommodate site-specific development applications.

Since 2004, there have also been numerous changes and updates to the provincial, regional and local planning policy documents necessitating a comprehensive update of the Secondary Plan. This includes the Lake Simcoe Protection Plan (2009), York Region Official Plan (2010), South Georgian Bay

Lake Simcoe Source Protection Plan (2015), Town of Georgina Official Plan (2016), Greenbelt Plan (2017), Growth Plan for the Greater Golden Horseshoe (2019), and the Provincial Policy Statement (2020). This update was largely undertaken in the context of and concurrently with, the York Region Municipal Comprehensive Review and the preparation of the new Regional Official Plan, which has informed the growth projections to the 2041 planning horizon, while also implementing the updated Regional planning policy framework.

Amendment No. 148 deletes the existing Keswick Secondary Plan (2004) and replaces it with a new Keswick Secondary Plan (2022) that sets out the vision for the future growth, development and redevelopment of the community of Keswick. The new Secondary Plan includes specific land use designations, mapping and policies directing the type and character of development that may occur, environmental protection, municipal servicing requirements, and transportation and other policies to manage change in the community to 2041.

The primary purpose of the Keswick Secondary Plan is to manage growth and development, ensuring a high quality of life for present and future residents, and the provision of a range of housing opportunities, parkland and recreational space, access to goods and services, enhanced natural features, and expanded employment opportunities.

The Council of the Corporation of the Town of Georgina is satisfied that Amendment No. 148 is appropriate.

PART C – THE AMENDMENT

1. INTRODUCTION

The whole of that part of the Amendment entitled "Part C – The Amendment", which consists of the following explanatory text constitutes Amendment No. 148 to the Town of Georgina Official Plan (Keswick Secondary Plan).

2. ACTUAL AMENDMENT

That the Official Plan of the Town of Georgina, as amended, be further amended as follows:

- 1) That the reference to the Schedules and Appendices of the Keswick Secondary Plan in Section 13.1 of the Town of Georgina Official Plan be revised to read:

Schedule A: Growth Management
Schedule B: Land Use Plan
Schedule C: Environmental Overlays
Schedule D: Source Water Protection Areas
Schedule E: Transportation
Schedule F: Site-Specific Exceptions

Appendix I: Urban Design and Architectural Control
Guidelines

Appendix II: Natural Environmental Background Report
Mapping”

- 2) That the text of Section 13.1, being the existing “The Keswick Secondary Plan” and Schedules F1, F2, F3, F4 and F5, be deleted in its entirety and replaced with a new Section 13.1, “The Keswick Secondary Plan”, that consists of the attached text along with Schedules A, B, C, D, E and F, forming Attachment ‘A’ to this Amendment.

3. IMPLEMENTATION

The Keswick Secondary Plan will be implemented in accordance with Section 11, Implementation, of the Town of Georgina Official Plan and Section 13.1.8, Implementation, of the Keswick Secondary Plan.

4. INTERPRETATION

The provisions set forth in Section 12, Interpretation, of the Town of Georgina Official Plan and Section 13.1.9, Interpretation, of the Keswick Secondary Plan, as amended from time to time regarding the interpretation of that Plan, shall apply in regard to this Amendment.

READ and **ENACTED** this 7th day of September, 2022.

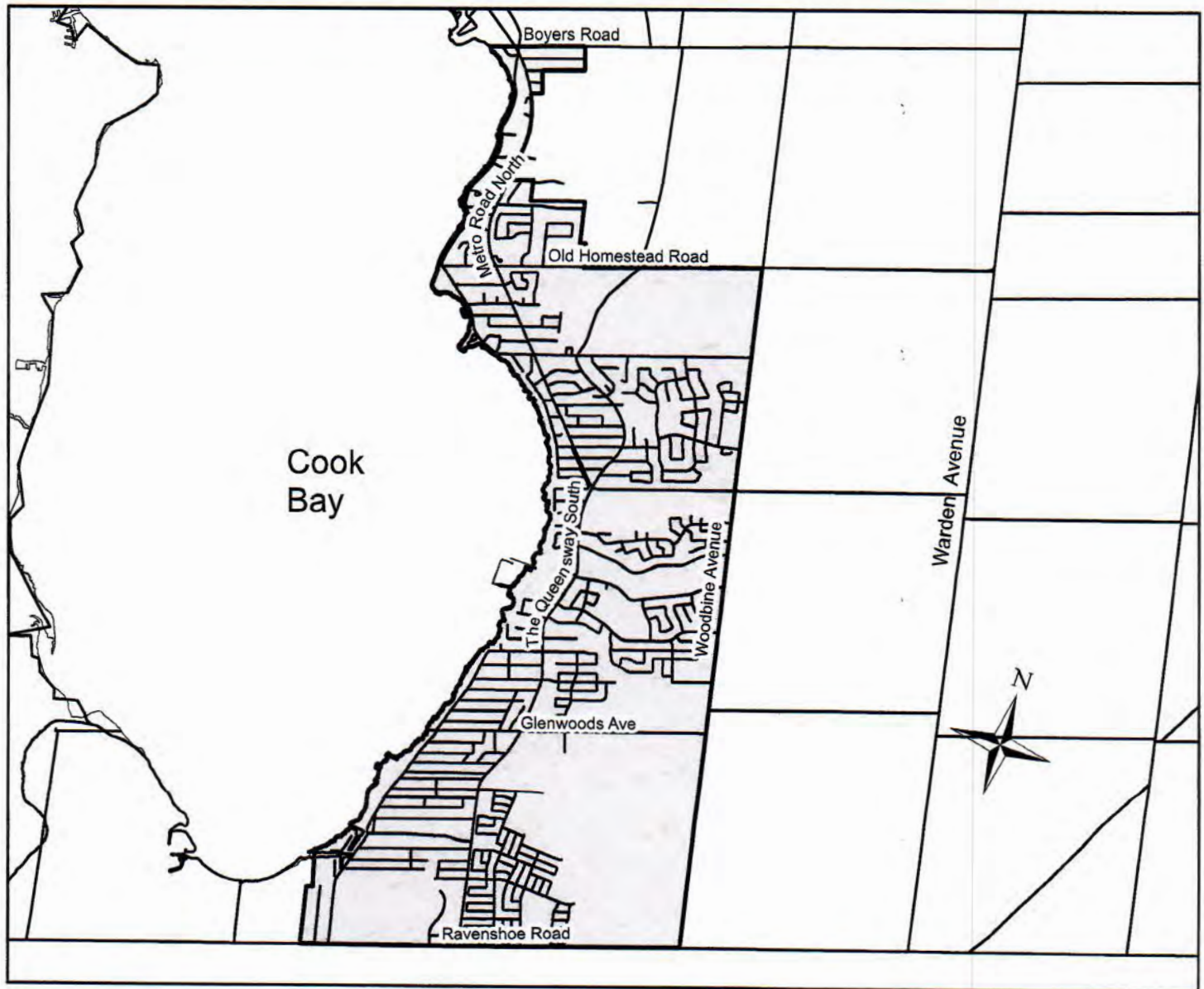
Margaret Quirk, Mayor

Mamata Baykar, Deputy Clerk

SCHEDULE 'A' - LOCATION MAP

Official Plan Amendment No. 2022-0088 (PL-2)

The lands within the limit of the bold outline on the map below are identified as "Keswick" on Schedule A2 to the Town of Georgina Official Plan and shall be subject to the text, schedules and appendices of the Keswick Secondary Plan that is being adopted through this Amendment.



SUBJECT LAND DESCRIPTION:

The Community of Keswick which is generally bounded by the Lake Simcoe Shoreline to the west, Metro Road North and Woodbine Avenue to the east, Boyers Road and Old Homestead Road to the north and Ravenshoe Road to the south.

LAND USE DESIGNATIONS:

Urban Centres, Mixed-Use Corridor 1, Mixed-Use Corridor 2, Existing Neighbourhood, New Neighbourhood, Tourist Commercial, Institutional/Community, Environmental Protection Area and Open Space.

KESWICK SECONDARY PLAN

August 2022



TABLE OF CONTENTS

13.1.1 BASIS OF THIS SECONDARY PLAN.....	1
13.1.1.1 Purpose of this Secondary Plan	1
13.1.1.2 Planning Area and Relationship with the Keswick Business Park.....	1
13.1.1.3 Application of this Secondary Plan	1
13.1.2 VISION AND GUIDING PRINCIPLES.....	4
13.1.2.1 Community Vision	4
13.1.2.2 Guiding Principles	4
13.1.3 GROWTH MANAGEMENT	5
13.1.3.1 Planned Urban Structure	5
13.1.3.2 Population and Employment Growth	6
13.1.4 BUILDING A COMPLETE KESWICK.....	8
13.1.4.1 A Healthy and Accessible Community.....	8
13.1.4.1.1 Objectives of a Healthy and Accessible Community	8
13.1.4.1.2 Policies to Promote a Healthy and Accessible Community	8
13.1.4.2 A Strong Economy	11
13.1.4.2.1 Objectives of a Strong Economy	11
13.1.4.2.2 Policies to Promote a Strong Economy	12
13.1.4.3 An Attractive and High Quality Community	13
13.1.4.3.1 Objectives of an Attractive and High Quality Community	13
13.1.4.3.2 Policies to Promote an Attractive and High Quality Community	13
13.1.4.4 A Sustainable and Resilient Community	16
13.1.4.4.1 Objectives of a Sustainable and Resilient Community	16
13.1.4.4.2 Policies to Promote a Sustainable and Resilient Community	16
13.1.5 GENERAL LAND USE POLICIES.....	19
13.1.5.1 Land Uses Permitted in all Designations.....	19
13.1.5.2 Land Uses Prohibited in All Designations.....	19
13.1.5.3 Residential Land Use Policies	19
13.1.5.3.1 Low-Rise Residential Uses.....	19
13.1.5.3.2 Mid-Rise Residential Uses	20
13.1.5.3.3 High-Rise Residential Uses.....	20
13.1.5.3.4 Additional Residential Units.....	21
13.1.5.3.5 Special Needs Housing	22
13.1.5.3.6 Home Occupations	23

Keswick Secondary Plan

13.1.5.3.7	Live-Work Units	24
13.1.5.3.8	Short-Term Rental Accommodation	24
13.1.5.4	Public Service Facilities.....	24
13.1.5.4.1	Emergency Services.....	25
13.1.5.4.2	Education Facilities.....	25
13.1.6	LAND USE DESIGNATIONS	26
13.1.6.1	Local Strategic Growth Area.....	26
13.1.6.1.1	Urban Centres Designation	26
13.1.6.1.2	Mixed-Use Corridor 1 Designation – The Queensway	29
13.1.6.1.3	Mixed-Use Corridor 2 Designation – Woodbine Avenue	30
13.1.6.2	Neighbourhoods	32
13.1.6.2.1	Existing Neighbourhood Designation	32
13.1.6.2.2	New Neighbourhood Designation	34
13.1.6.2.3	Neighbourhood Supporting Uses	35
13.1.6.3	Community Supporting Uses.....	36
13.1.6.3.1	Tourist Commercial Designation.....	36
13.1.6.3.2	Institutional/Community Designation	36
13.1.6.3.3	Day Care Facilities	37
13.1.6.3.4	Places of Worship.....	38
13.1.6.4	The Natural Heritage System and Parks Network.....	38
13.1.6.4.1	Environmental Protection Area Designation	38
13.1.6.4.2	Environmental Overlays	41
13.1.6.4.3	Requirements for an Environmental Impact Study	43
13.1.6.4.4	Parks and Open Space Designation	44
13.1.6.5	Site-Specific Exceptions	46
13.1.7	PROVIDING SUSTAINABLE SERVICES AND INFRASTRUCTURE	51
13.1.7.1	The Transportation System	51
13.1.7.1.1	General Transportation Policies	51
13.1.7.1.2	Road Network.....	52
13.1.7.1.3	Active Transportation System.....	57
13.1.7.1.4	Public Transit System.....	58
13.1.7.1.5	Parking Management	58
13.1.7.2	Sanitary Sewage and Water Supply Services	59
13.1.7.3	Stormwater Management	62
13.1.7.4	Communication Technology	65

13.1.8 IMPLEMENTATION	66
13.1.8.1 Authority	66
13.1.8.2 Existing Non-Conforming Uses, Buildings and Structures	66
13.1.8.3 By-Laws.....	66
13.1.8.4 Plans of Subdivision/Condominium and Consents.....	66
13.1.8.4.1 Plans of Subdivision/Condominium	66
13.1.8.4.2 Consents	67
13.1.8.5 Site Plan Control	68
13.1.8.6 Community Improvement	68
13.1.8.7 Land Dedication, Acquisition and Securement.....	68
13.1.8.8 Asset Management Plan and Capital Works Program	68
13.1.8.9 Site Alteration	69
13.1.8.10 Contaminated Lands.....	69
13.1.8.11 Municipal Finance	69
13.1.8.11.1 Fiscal Responsibility	69
13.1.8.11.2 Development Charges.....	69
13.1.8.11.3 Other Financial Requirements	70
13.1.8.11.4 Developers Group Agreement.....	70
13.1.8.12 Development Review	70
13.1.8.12.1 Amendments to this Secondary Plan	70
13.1.8.12.2 Pre-Consultation and Submission Requirements.....	71
13.1.8.12.3 Complete Application Requirements	71
13.1.8.12.4 Peer Review	71
13.1.8.12.5 Public Consultation.....	71
13.1.8.12.6 Decision Making	71
13.1.8.13 Monitoring and Measuring Performance.....	72
13.1.8.14 Phasing	72
13.1.9 INTERPRETATION	73
13.1.9.1 Transition.....	73
13.1.9.2 Land Use Boundaries and Roads	73
13.1.9.3 Stated Measurements and Quantities	73
13.1.9.4 Legislation	74
13.1.9.5 Definitions.....	74

Tables

Table 1: Keswick Population and Employment Forecasts

Appendix

Appendix I: Urban Design and Architectural Control Guidelines

Appendix II: Natural Environment Background Report Mapping

13.1.1 BASIS OF THIS SECONDARY PLAN

The following text, schedules identified as Schedules A through F, and Appendix I and II constitute the Keswick Secondary Plan.

13.1.1.1 Purpose of this Secondary Plan

This Secondary Plan replaces the Keswick Secondary Plan that was approved by York Region on October 5, 2004 and came into force and effect on October 26, 2004. The planning horizon for this Secondary Plan is to the year 2041.

The purpose of this Secondary Plan is to provide a detailed land use plan and policy framework to guide future growth and development within Keswick in accordance with provincial plans and policies, the York Region Official Plan, and the Town of Georgina Official Plan (Georgina Official Plan or Official Plan). This Secondary Plan implements a number of recent planning policy and legislative changes made by the Province, including:

- The Planning Act, as amended;
- Provincial Policy Statement, 2020;
- A Place to Grow: Growth Plan for the Greater Golden Horseshoe, 2020;
- Greenbelt Plan, 2017;
- South Georgina Bay Lake Simcoe Source Protection Plan, 2015; and,
- Lake Simcoe Protection Plan, 2009.

13.1.1.2 Planning Area and Relationship with the Keswick Business Park

Keswick encompasses an area of approximately 1,720 hectares (4,250 acres) and is the largest urban community in the Town of Georgina. Schedule A identifies the location of the Keswick Secondary Plan Area as well as the Keswick Business Park Secondary Plan Area located on the east side of Woodbine Avenue, adjacent to Keswick.

Although this Secondary Plan does not apply to the lands within the Keswick Business Park, the community of Keswick and the Keswick Business Park are functionally linked and together will ensure that Keswick evolves into a *complete community*, which includes the important *employment land jobs* that will be located within the Business Park. It is anticipated that over time the Keswick Business Park has the potential to accommodate between 7,500 and 9,000 jobs.

13.1.1.3 Application of this Secondary Plan

Land use, *development* and public works in Keswick shall conform to the vision, guiding principles, objectives, policies and schedules of this Secondary Plan. In addition, while the Urban Design and Architectural Control Guidelines attached as Appendix I do not form a statutory part of this Secondary Plan, they are meant to guide the preparation of detailed *development* plans in

accordance with the Town's vision for Keswick. It is the intent of this Secondary Plan that all new *development* have regard for the Urban Design and Architectural Control Guidelines, as well as other Council-adopted documents, including but not limited to:

- Development Design Criteria;
- Streetscape Design Standards;
- Tree Preservation and Compensation Policy;
- Trails and Active Transportation Master Plan;
- Recreation Facility Needs Study;
- Parkland Standards Manual;
- Parks, Facilities and Construction Standard Details; and,
- Other Council-adopted design guidelines and programs as introduced from time to time.

The Natural Environment Background Report, along with the mapping provided as Appendix II, provide the basis for the Natural Heritage System and the environmental policy framework contained in this Secondary Plan that will protect and conserve Keswick's significant natural heritage and hydrologic features and their associated ecological functions.

Development shall also conform with the relevant policies of the York Region Official Plan, the Lake Simcoe Protection Plan, the South Georgian Bay Lake Simcoe Source Protection Plan, the Greenbelt Plan and the Growth Plan for the Greater Golden Horseshoe and be consistent with the Provincial Policy Statement. Where this Secondary Plan makes references to an Act of the Legislature or an Ontario Regulation, such reference shall include its successor upon amendment or replacement.

This Secondary Plan is to be read in its entirety, and all relevant policies are to be applied to applications for *development* and public works. This Secondary Plan is also to be read in conjunction with the Georgina Official Plan, and all relevant policies of the Official Plan are to be applied to, and conformed with when considering any application for *development*, or making any land use planning decision. Where there is a conflict between the policies of the Georgina Official Plan and this Secondary Plan, the policies of this Secondary Plan shall apply.

This Secondary Plan contains policies which are geographically specific to Keswick, and refers to sections of the Georgina Official Plan which apply Town-wide. The following sections of the Official Plan are referenced in this Secondary Plan and are to be applied when implementing the Plan:

- Section 4.1: Land Uses Permitted in all Designations;
- Section 4.2: Land Uses Prohibited in all Designations;
- Section 4.3: Site Alteration;
- Section 4.4: Natural Hazards;
- Section 4.5: Contaminated Lands;
- Section 5.5: Source Water Protection;

Keswick Secondary Plan

- Section 8.6: Community Improvement;
- Section 8.7: Community Design;
- Section 8.8: Cultural Heritage and Archaeological Resources;
- Section 9.2.2: Development on Assumed Public Roads and Private Roads;
- Section 10.1.1: Pre-Consultation;
- Section 10.1.2: Submission Requirements;
- Section 10.1.3: Complete Application;
- Section 10.2: Peer Review;
- Section 11.1: Existing Non-Conforming Uses, Buildings and Structures;
- Section 11.2: Zoning By-laws;
- Section 11.3: Plans of Subdivision/Condominium;
- Section 11.5: Site Plan Control;
- Section 11.6.1: Development Charges;
- Section 11.6.2: Other Financial Requirements;
- Section 11.6.1: Development Charges;
- Section 11.7: Land Dedication, Acquisition and Securement;
- Section 11.11: Public Consultation;
- Section 11.13: Maintenance and Occupancy By-laws;
- Section 11.14: Municipal Act By-laws;
- Section 11.15: Asset Management Plan and Capital Works Program; and,
- Section 11.17: Monitoring and Measuring Performance.

This Plan also includes non-policy textual commentary which are located in grey shaded boxes throughout the document. This information is intended to provide additional context or to explain the intent of certain concepts or policies. This textual commentary is not to be afforded any independent status in interpreting this Secondary Plan and is to take on meaning only as an explanation of the policies.

13.1.2 VISION AND GUIDING PRINCIPLES

13.1.2.1 Community Vision

Keswick will become a more complete, healthy and vibrant community, balancing its existing lakeside character with new *development* that meets the community's employment, shopping and entertainment needs, and that provides more rental and *affordable* housing to support a diverse population. As Keswick evolves, new *development* and investment will prioritize the creation of a stronger sense of community, a well-connected and *multimodal* transportation network, the protection of natural areas and a commitment to environmental sustainability and resiliency.

13.1.2.2 Guiding Principles

The following guiding principles further articulate the vision and are the foundation upon which the objectives and policies of this Secondary Plan are based:

- PRINCIPLE 1:** To develop as a complete, healthy, attractive, safe, inclusive and accessible community for the present and future residents of Keswick;
- PRINCIPLE 2:** To ensure a balance of low, medium and high density and mixed-use development and *intensification* to meet the Town's growth targets and provide a full mix and range of housing options, including *affordable* and rental housing;
- PRINCIPLE 3:** To promote and strengthen community identity and cohesion by supporting mixed-use community nodes and corridors, a high quality public realm and stronger connections and public access to the Lake Simcoe shoreline;
- PRINCIPLE 4:** To ensure that new *development* contributes to building resiliency, reflects efficient land use patterns and mitigates the impacts of climate change;
- PRINCIPLE 5:** To ensure that new *development* is integrated with existing land uses in a logical, orderly and efficient manner, and is coordinated with planning for transportation and municipal services;
- PRINCIPLE 6:** To ensure the provision of an accessible, efficient, connected and *multimodal* transportation network, that supports pedestrian-oriented communities and gives priority to the creation of *complete streets* and the provision of *active transportation* and transit infrastructure;
- PRINCIPLE 7:** To ensure that Keswick develops in a manner that promotes a competitive and adaptable economic environment that protects for future employment generating activities, encourages investment, provides a diversity of business and employment opportunities and incorporates high quality broadband connectivity; and,
- PRINCIPLE 8:** To protect the health and connectivity of Lake Simcoe and the Natural Heritage System and Parks Network, including public parks, open space and natural heritage and hydrologic features and their functions.

13.1.3 GROWTH MANAGEMENT

13.1.3.1 Planned Urban Structure

- a) Schedule A provides the planned urban structure of Keswick. It identifies the following key geographical components of the community:
- i) **The Settlement Area Boundary** – The Settlement Area Boundary establishes the jurisdictional extent of this Secondary Plan;
 - ii) **The Urban Service Area Boundary** – *Development* on lands within the Urban Service Area Boundary are to be serviced by municipal sewer and water supply services. The Urban Service Area Boundary coincides with the Settlement Area Boundary;
 - iii) **The Natural Heritage System and Parks Network** – The Natural Heritage System and Parks Network provides an important structural element throughout Keswick, and includes lands designated Environmental Protection Area and Parks and Open Space;
 - iv) **The Delineated Built-up Area** – The *Delineated Built-up Area* was defined in 2006 by the Province in order to identify those areas within the Settlement Area Boundary where the *intensification* target is to be measured;
 - v) **The Designated Greenfield Area** – The *Designated Greenfield Area* includes those areas within the Settlement Area Boundary that are outside of the *Delineated Built-Up Area*;
 - vi) **The Local Strategic Growth Area** – The *Local Strategic Growth Area* are lands which have been identified as the focus for accommodating *intensification*, higher-density and a mix of uses in a more compact built form; and,
 - vii) **Neighbourhoods** – The Neighbourhoods are lands which are envisioned to be predominantly a mix of low-rise and mid-rise residential areas, providing a range of *housing options* for current and future residents.

13.1.3.2 Population and Employment Growth

The majority of the employment within the Keswick Secondary Plan Area is forecast to be *population-related employment*, which will primarily include a range of retail and service commercial uses, institutional uses and business and professional offices. Planned provincial highway infrastructure, including the construction of the Highway 404 extension to the east of Keswick, and the Highway 400 - Highway 404 connecting link to the south, have the potential to enhance the area's accessibility and attractiveness as both a place to live and as a place to locate a business. With this potential in mind, subsequent reviews of this Secondary Plan will consider the Regional allocation of population and employment projections for Georgina.

Fundamental to Keswick's long-term ability to accommodate projected growth is the provision of municipal infrastructure, including sewage treatment facilities and an adequate water supply. York Region owns and maintains sewage treatment and water supply facilities that are required to service the planned population and employment growth in Keswick.

Growth Allocations and Phasing

- a) As set out in Table 1, the estimated population and employment¹ growth forecast in Keswick is as follows:

Table 1: Keswick Population and Employment Forecasts*

	2021	2031	2041
Total population (people)	29,820	37,090	41,090
Total employment (jobs)	5,260	6,210	6,970

**The forecasts in Table 1 are based on York Region's draft population and employment forecast as endorsed by Town Council on June 23, 2021. This forecast represents an average annual growth of 213 new dwelling units per year based on a persons per unit of 2.64. These forecasts are considered to be targets and it is recognized that there are many factors that affect the pace of growth and, therefore, the timing and rate of growth may vary or differ from what is projected over this time horizon. As this Secondary Plan is periodically reviewed, the growth forecast may need to be adjusted or updated.*

- b) Population and employment growth in Keswick will be phased based on the sewage treatment capacity of the Keswick Water Resource Reclamation Facility.
- c) The Environmental Assessments approved for the Keswick Water Resource Reclamation Facility allow for a sanitary servicing capacity equivalent to 49,000 (Phase I) and 66,000 (Phase II) persons, to service existing development and new growth in Keswick, new *development* within the Keswick Business Park, and existing *development* and limited new

¹ Only commercial and institutional uses with overnight accommodations require servicing allocation assignment.

growth in the Serviced Lakeshore Residential Area located along the Lake Simcoe shoreline between Keswick and Jackson's Point.

- d) The Phase I share of existing capacity from the Keswick Water Resource Reclamation Facility allocated to service the existing *development* and new growth in Keswick is 36,790 persons equivalent, while the Phase II Environmental Assessment approved expansion of the Keswick Water Resource Reclamation Facility will provide a total sanitary servicing capacity of 50,600 persons equivalent to service Keswick.

Intensification within the Delineated Built-Up Area

- e) This Secondary Plan requires that a minimum of 28% of all new residential *development* within Keswick shall occur within the *Delineated Built-up Area* to 2041. Such *intensification* will be subject to the following policies:
 - i) *Intensification* will be primarily accommodated within the *Local Strategic Growth Area* in the *Delineated Built-up Area*; and,
 - ii) *Intensification* within the Existing Neighbourhood designation is anticipated to be modest, while those areas continue to evolve.

Development within the Designated Greenfield Area

- f) The *Designated Greenfield Area* is expected to accommodate significant growth as it develops into new neighbourhoods and mixed-use areas.
- g) To 2041, the *Designated Greenfield Area* is planned to achieve an overall density of 50 residents and jobs combined per gross hectare. Upon full buildout of the *Designated Greenfield Area*, it is estimated to accommodate approximately 18,750 people and jobs combined.
- h) To achieve the overall density target in the *Designated Greenfield Area*, *development* will be subject to the following policies:
 - i) There shall be an appropriate mix of low-rise, mid-rise and high-rise residential uses distributed throughout the Designated Greenfield Area; and,
 - ii) The Town in consultation with York Region shall monitor the composition and distribution of the proposed housing stock and anticipated jobs within the *Designated Greenfield Area*. To facilitate this monitoring, the proponent shall demonstrate through the *development* review process how the proposal contributes toward the achievement of an overall density target of 50 persons and jobs combined per gross hectare.

13.1.4 BUILDING A COMPLETE KESWICK

A *complete community* provides opportunities for people of all ages and abilities to conveniently access most of their daily needs throughout their lives within their community. To support Keswick in becoming a *complete community*, the following objectives and related policies shall be complied with when making decisions on *development* applications and municipal works.

13.1.4.1 A Healthy and Accessible Community

Physical, social and mental well-being are some of the necessary components of public health, including access to healthy food, clean air and water, and opportunities for physical activity. The built environment should be designed to create opportunities to encourage residents to be physically active and socially engaged. A fundamental element of a healthy community is *active transportation*, which refers to any form of human-powered transportation. An accessible community ensures that residents of all ages, abilities and means can easily meet their needs for daily living by providing convenient access to an appropriate mix of uses, a well-developed open space system, public transportation, and options for *active transportation*.

13.1.4.1.1 Objectives of a Healthy and Accessible Community

- a) An *active transportation* system that ensures pedestrian safety, comfort, and accessibility for all, while providing connectivity within the community, the Town in general, and to transportation systems that serve the broader region, and that is in accordance with the Town's Trails and Active Transportation Master Plan.
- b) To support year-round pedestrian activity within the community by providing housing in close proximity to a substantial number of destinations including parks and open spaces, cultural facilities, *public service facilities*, retail shops and restaurants.
- c) To support and provide "age-in-place" facilities and housing within the community that anticipate changing lifestyle needs for an aging population.
- d) To accommodate a range and mix of housing types, sizes and tenures, including *affordable* and rental housing, to allow for a range of choice and opportunity for residents. This includes higher density built forms of development as a way of reducing land consumption and maximizing the efficiency and cost effectiveness of municipal service infrastructure. Higher density forms of housing will assist to increase "age-in-place" and *affordable housing options*.

13.1.4.1.2 Policies to Promote a Healthy and Accessible Community

Ensuring a Range and Mix of Housing

- a) The Town will work with York Region, the private sector and other stakeholders to provide a full range of housing types and densities to meet the projected demographic and market requirements of the current and future residents of Keswick. The Town shall encourage a range of housing types, styles, tenure and affordability to meet the needs of a diverse

population. The Town may become directly involved in the supply of housing through land acquisitions and development partnerships.

- b) An appropriate range and mix of housing types and densities is required to meet the projected housing needs of current and future residents of Keswick and shall be consistent with Regional forecasts, and *intensification* and density requirements. The Town will ensure an adequate housing supply in Keswick by:
 - i) Maintaining a minimum ten-year supply of land to accommodate residential growth through *intensification* and *redevelopment*, and lands which are designated and available for residential *development*; and,
 - ii) Maintaining a minimum three-year supply of residential units available through lands suitably zoned to facilitate residential *intensification* and *redevelopment*, and land in draft approved and registered plans.
- c) The Town will encourage and support the construction of new rental units with a full mix and range of unit sizes, including family-sized and smaller units in appropriate locations.
- d) In undertaking the review of Planning Act applications, the Town shall protect rental housing from demolition and conversion to condominium or non-residential uses by prohibiting demolitions or conversions that would result in a rental vacancy rate of less than 3% in the Town. In this regard, the applicant shall submit a rental vacancy rate study for approval by the Town.
- e) The Town shall ensure a diversity of housing options are available to residents of all ages, abilities and incomes using planning tools such as:
 - i) Plan of subdivision and/or condominium process;
 - ii) Zoning By-law regulations;
 - iii) Development agreements;
 - iv) *Community Improvement Plans*;
 - v) Site Plan Control;
 - vi) Council approved incentive programs; and,
 - vii) The allocation of municipal sewage treatment and water supply capacity.
- f) The housing supply objectives of this Plan are contingent upon the ability of York Region, in coordination with the Town, to adequately expand sewage treatment and water supply facilities in a timely manner.

Delivering Affordable Housing

- g) It is a target of this Secondary Plan that on an annual basis a minimum of 25% of new housing units within Keswick be *affordable*.
- h) Policies for the provision of *affordable* housing in Keswick are as follows:

- i) *Affordable* housing must include a mix and range of building types, lots sizes, unit types/sizes, and tenures to provide opportunity for all household types, including larger families and residents with special needs; and,
- ii) Identifying optimal sites, including prioritizing publicly-owned lands for *affordable* housing, early in the *development* process to maximize *affordable* housing funding opportunities in consultation with the building industry, *community housing* agencies and other stakeholders. This includes:
 - Working with York Region, the development industry, non-profit groups and other housing related organizations to achieve *affordable* housing targets;
 - Encouraging the creation of intrinsically *affordable* housing, which includes modest amenities, standard materials, minimal details and flexibility within units;
 - Support for the creation of *additional residential units* as a key component of *intensification* throughout Keswick's existing neighbourhoods; and,
 - Encouraging the location of *affordable* housing to be in proximity to transit and other human services.
- i) The Town will consider innovative financial arrangements, tools, policies and partnerships to encourage the private sector and other stakeholders in the development and maintenance of *community housing* and *affordable* housing, such as:
 - i) Pre-zoning identified sites;
 - ii) The allocation of municipal sewage treatment and water supply capacity;
 - iii) *Community Improvement Plans*;
 - iv) Council approved incentive programs;
 - v) Deferral of development charges; and,
 - vi) Allowing more modest amenities, standards and material, minimal details and flexibility within units.
- j) The Town shall encourage, as a component of the *affordable* housing target, the provision of housing that is cost-subsidized (i.e. *community housing*) and that is necessary to meet the needs of households unable to find *affordable* housing through the private market. Such *community housing* may be provided by the public or private sector, and may be in conjunction with senior government programs.

Delivering Accessible Housing Forms

- k) The Town shall work with appropriate agencies to ensure an adequate supply of appropriately scaled accessible housing forms, including:
 - i) *Special needs housing*, including *group homes*, *lodging houses*, *halfway houses*, *homes for special care* and *senior care facilities*;
 - ii) *Community housing*;

- iii) *Emergency housing*;
 - iv) *Rooming houses and boarding houses*; and,
 - v) Innovative housing models such as *co-housing*.
- l) Appropriately scaled accessible housing forms shall be prioritized in the Urban Centres, and Mixed-Use Corridors where *public service facilities* and amenities are easily accessible to ensure residents with special needs are able to care for themselves as much as possible in a non-institutional setting.
- m) The design and construction of new housing should consider the installation of accessibility features, as appropriate.

Monitoring the Housing Supply

- n) The Town, in conjunction with York Region, shall monitor, evaluate and regularly report on changes within the housing market and progress made in achieving the housing targets of this Plan. This includes:
- i) The inventory of vacant lands and lands with the potential for residential *development*;
 - ii) The *affordable* housing targets identified within this Secondary Plan;
 - iii) Residential construction activity, including the tenure and type of housing being developed; and,
 - iv) Available servicing capacity for residential *development* to ensure that the housing targets of this Secondary Plan can be achieved.

13.1.4.2 A Strong Economy

A strong economy is market responsive, efficient and cost effective. Ongoing and enhanced economic development is a fundamental prerequisite to Keswick's future success and sustainability. The attraction of new businesses, jobs, investment and tourism are key elements of the vision for Keswick.

13.1.4.2.1 Objectives of a Strong Economy

- a) To attract long-term economic *development* and investment by creating opportunities for local businesses and commercial activities to thrive.
- b) To become a leader in attracting new economic drivers by adapting to trend changes.
- c) To provide a full range of *population-related employment* options to provide local jobs and adequately service the community.
- d) That *development* is cost-effective, financially viable over its life cycle, and appropriate for the context of the community and economic market.

- e) To optimize the use of existing infrastructure investments and to promote the coordinated, efficient and cost-effective delivery of service and community infrastructure, including fast and affordable broadband connectivity, that is appropriate for the *development* directed by this Plan.

13.1.4.2.2 Policies to Promote a Strong Economy

Enhance the Economy

- a) To help attract and retain a diverse and skilled labour force, the Town will support a strong and healthy economy by:
 - i) Designating adequate lands to provide opportunities for the expansion and diversification of Keswick's retail and service commercial sector;
 - ii) Constructing, upgrading and maintaining quality municipal infrastructure systems and *public service facilities*;
 - iii) Facilitating efficient and convenient transportation options for the movement of people and goods;
 - iv) Supporting options for *live-work units* and a range of *home occupations*, including the provision of high quality broadband connectivity;
 - v) Supporting the provision of broad range of *housing options* affordable to all income levels; and,
 - vi) Supporting small businesses and entrepreneurship opportunities.

Expand and Diversify Commercial Uses

- b) The Town will continue to diversify its economic base by supporting its evolving urban structure, which includes a full range of *population-related employment* generating uses, including small to mid-scale offices, institutional uses, and a variety of retail and service commercial businesses.
- c) The lands designated for mixed-use development adjacent to Woodbine Avenue and The Queensway shall accommodate a complete range of commercial businesses and services to foster competition and choice.
- d) Opportunities for the intensification and revitalization of existing retail and service commercial businesses are encouraged and supported in appropriate locations.

Promote Tourism

- e) The Town promotes tourism as an integral part of economic development and recognizes that tourism will bring economic benefits to Keswick. Tourism is supported by having a healthy, livable and diverse community, including vibrant Urban Centres, cultural and recreational facilities and access to the waterfront, which is all connected by an integrated transportation system.
- f) To further support water-based tourism, such as sport fishing, boating, swimming, ice fishing and snowmobiling, opportunities should be identified to:

- i) Improve public access to the Lake Simcoe shoreline;
 - ii) Improve existing marina facilities through potential Public-Private Partnerships and joint funding opportunities and/or the *development* of new marinas; and,
 - iii) Encourage year-round access, parking, accommodation facilities and related service commercial and restaurant uses to support water-based tourism activities.
- g) The *development* of more accommodation facilities for visitors will be promoted as part of the effort to continue improving the Town's profile as a tourist destination.

13.1.4.3 An Attractive and High Quality Community

An attractive and high quality community includes well designed buildings, public spaces and streetscapes. This includes the creation of destinations, landmarks and gateways that distinguish the community and establish a sense of place. Crucial to an attractive and high quality community is the attention to the interplay between built form, the public realm and the natural environment.

13.1.4.3.1 Objectives of an Attractive and High Quality Community

- a) To welcome, encourage and support active living, social engagement, and civic pride. Fundamental to achieving this objective is enhancing year-round connections and public access to the Lake Simcoe shoreline, and the creation of public spaces that create a sense of place and well-being.
- b) That roads within Keswick be planned, designed and operated as *complete streets*.
- c) That public parks and open spaces are designed to be accessible and accommodate a variety of user groups and activities. Where possible and appropriate, public parks and open spaces should be integrated with *key natural heritage features* and *key hydrologic features* and be connected through *active transportation* routes.
- d) That new *development* be *compatible* with existing development, while also including a diversity of architectural styles and elements.

13.1.4.3.2 Policies to Promote an Attractive and High Quality Community

Urban Design and Architectural Control

- a) Urban design and architectural control in Keswick shall be regulated in accordance with the policies of Section 8.7 of the Georgina Official Plan. In addition to the policies of the Official Plan, Policies 13.1.4.3.2(b) to 13.1.4.3.2(j) of this Plan also apply to urban design and architectural control in Keswick.
- b) All *development* shall have regard for the Urban Design and Architectural Control Guidelines attached to this Secondary Plan as Appendix I. The purpose of the Urban Design and Architectural Control Guidelines is to assist in the achievement of the Secondary Plan policies, to provide criteria for the evaluation of development applications, and to serve as the foundation for site-specific Design Guidelines.

Compatible Development

- c) The principle of *compatible development* will be applied to all applications for development to ensure the sensitive integration of new development.
- d) All new *development* within Keswick shall be *compatible* and sensitively integrated with the surrounding land uses in terms of building design, mass and character, height, setbacks, orientation, privacy, landscaping, shadow casting, accessibility and visual impact.
- e) Within the framework of *compatible development*, the Town supports and encourages architecture and design approaches that interpret and enhance the built environment and thereby creates a 21st century built form.
- f) To ensure *development* is *compatible*, the following criteria shall be ensured:
 - i) The landscape, built form and functional character of the surrounding community is maintained or enhanced;
 - ii) No undue, *adverse effects* are created on adjacent properties;
 - iii) Natural heritage and hydrologic features and their functions are appropriately protected;
 - iv) Identified on-site or adjacent *cultural heritage resources* are appropriately conserved and *development* is *compatible* with their character;
 - v) The height and massing of nearby buildings is appropriately considered, and buffers and/or transitions in height to adjacent properties are implemented, where necessary;
 - vi) On-site amenity space is provided and is *compatible* with the existing patterns of private and public amenity space in the vicinity; and,
 - vii) *Development* is *compatible* with streetscape patterns, including block lengths, setbacks and building separations.
- g) Where new *development* is proposed within or abutting the Existing Neighbourhood or New Neighbourhood designations, the following design considerations may be utilized to ensure such *development* is *compatible* and that there is an appropriate transition of built form:
 - i) Additional setbacks;
 - ii) *Angular plane* projections;
 - iii) Enhanced landscaping and/or fencing; and,
 - iv) Location of servicing, loading and parking areas.
- h) *Development* shall be planned and designed to avoid, or if avoidance is not possible, minimize and mitigate any potential *adverse effects* from odour, noise and other contaminants, minimize risk to public health and safety, and to ensure the long-term operational and economic viability of *major facilities* in accordance with provincial guidelines, standards and procedures.

- i) Health and Air Quality Impact Studies that assess the potential impact on human health from proposed development near *sensitive land uses* may be required to support such *development*. The requirement for such studies shall be determined by Town in consultation with York Region.

Crime Prevention Through Environmental Design

- j) Town requires building and site design that assists in the reduction of the incidence of crime through the implementation of Crime Prevention Through Environmental Design principles including:
 - i) Adequate lighting;
 - ii) Clear sight lines, allowing natural surveillance of open spaces and walkways;
 - iii) Avoiding landscaping, buildings and other design features that create blind spots or hiding places;
 - iv) Adequate fencing and fenestration; and,
 - v) Streetscape and building design that promotes interest, activity and 'eyes on the street'.

Cultural Heritage and Archaeological Resource Conservation

- k) Cultural heritage and archaeological resource conservation shall be regulated in accordance with the policies of Section 8.8 of the Georgina Official Plan. In addition to the policies of the Official Plan, when reviewing Planning Act applications, demolition permits, or the provision of utilities affecting lands or properties which are designated or adjacent to a designated cultural heritage resource, the Town will ensure that the proposal is *compatible* by:
 - i) Respecting the massing, cladding materials, profile and character of adjacent heritage resources;
 - ii) Having regard for the massing of nearby heritage buildings and its impact on the streetscape when constructing new buildings facing the street;
 - iii) Reflecting the established setback pattern on the street;
 - iv) Orienting the building to the street in a similar fashion to existing heritage buildings;
 - v) Ensuring that parking facilities are compatibly integrated into heritage areas by locating them to the rear of the property, where appropriate; and,
 - vi) Requiring local utility companies to place metering equipment, transformer boxes, power lines, conduit equipment boxes, and other utility equipment and devices in discrete locations that do not detract from the visual character or architectural integrity of the heritage resource.

13.1.4.4 A Sustainable and Resilient Community

A sustainable community uses its resources to meet current needs while ensuring that adequate resources are available for future generations. Sustainable communities also strive to provide a better quality of life for its residents while respecting the natural environment through such actions as minimizing waste, preventing pollution, promoting efficiency and protecting local resources. A resilient community is planned and designed to withstand and recovery from the impacts of adverse natural weather events.

13.1.4.4.1 Objectives of a Sustainable and Resilient Community

- a) Protection and enhancement of natural features and their associated environmental functions. Where removal of natural features is unavoidable, adequate compensation or replacement shall be provided.
- b) New *development* incorporate sustainability measures and green building design technology such as *Low Impact Development* and other site design strategies to mitigate environmental impacts and to create a more comfortable urban environment.
- c) Compact communities with a mix of uses and higher density built forms, connected by a transportation system that reduces reliance on the vehicle by promoting the use of public transit and *active transportation*.
- d) That *development* and infrastructure are designed in a way that mitigates and adapts to the health impacts of climate change and related extreme weather events such as flooding, heat waves, extreme cold and drought.

13.1.4.4.2 Policies to Promote a Sustainable and Resilient Community

Sustainability

- a) The Town shall use its array of planning tools, including this Secondary Plan and the associated Urban Design and Architectural Control Guidelines, to promote and guide *development* to be more sustainable.
- b) The Town may consider the use of *Community Improvement Plans* and associated incentive programs to assist with the implementation of sustainable development design standards.
- c) The Town will work with other levels of government, agencies and stakeholders to develop a Municipal Sustainability Plan.

Climate Change Mitigation and Adaptation

- d) The Town will work to integrate climate change mitigation and adaptation strategies through land use and development patterns that are sustainable, including the following initiatives:
 - i) Maximizing vegetation and natural areas to support improved air quality and carbon sequestration;
 - ii) Reducing emissions of greenhouse gases and other pollutants;
 - iii) Maximizing opportunities for the use of renewable energy systems;
 - iv) Promoting compact built form, a mix of uses and higher densities; and,
 - v) Promoting the use of *active transportation* and reduced vehicle trips.

Water Conservation

- e) Where practical and feasible, the Town encourages new *development* to achieve water efficiency and conservation targets that exceed Ontario Building Code requirements, including but not limited to the installation of rainwater harvesting and re-circulation/reuse systems within new buildings.

Energy Conservation

- f) The Town strongly supports and encourages *development* that minimizes energy consumption, maximizes solar energy gains, and makes use of renewable, on-site generation and district energy options including but not limited to solar, wind, biomass, and geothermal energy.
- g) New buildings are encouraged to include renewable energy sources and be designed to support net-zero energy systems. Partnerships and projects supporting net-zero energy systems and related initiatives are encouraged.
- h) *Development* proposals that include energy efficient building design and practices such as maximizing solar energy gains, increased building insulation, green roofs and *cool roofs*, shall be encouraged and supported.
- i) The maximization of solar energy gains may be achieved through design considerations such as:
 - i) Building and road orientation;
 - ii) Maximizing the number of south facing windows; and,
 - iii) Building construction that facilitates future solar installations (i.e. solar ready – built with all the necessary piping and equipment that would be needed to install a rooftop solar power system).

Tree Preservation and Compensation

- j) Canopy cover is calculated as a percentage of the total land area covered by all trees and shrubs. According to York Region's monitoring, canopy cover within Keswick as of 2019 is 25%. The Town will attempt to meet or achieve a minimum target of 30% tree canopy cover within Keswick by 2041. To achieve this, the Town will:
 - i) Review and update its Tree Preservation and Compensation Policy;
 - ii) Encourage and support the protection of the existing tree canopy and measures that support increased tree canopy cover; and,
 - iii) Implement street tree and naturalization programs to increase canopy cover.
- k) The protection and enhancement of tree cover is essential. The Town will support tree conservation by:
 - i) Ensuring existing trees are protected whenever possible, adequately replaced, or compensated for during the *development* process as described in the Town's Development Design Criteria and Tree Preservation and Compensation Policy;
 - ii) Maximizing the number of trees that can be conserved or planted on *development* sites;
 - iii) Fostering partnerships with York Region, the Lake Simcoe Region Conservation Authority and other stakeholder groups to increase planting initiatives;
 - iv) Promoting the planting of trees in suitable locations within the rights-of-way of Town and Regional roads; and,
 - v) Encouraging reforestation and shoreline maintenance along watercourses and the Maskinonge River to reduce flooding and soil erosion, and to provide fish and wildlife habitat.
- l) Any *development* application on a site which includes a woodlot, hedgerow, trees, or other woody vegetation shall be supported by a Tree Preservation and Compensation Plan in accordance with the Town's Tree Preservation and Compensation Policy, as revised or replaced.

Local Food Production

- m) *Development* plans and building designs should consider opportunities for local food growing and production through:
 - i) Community/shared gardens;
 - ii) *Edible landscapes*;
 - iii) Small scale food processing (i.e. community kitchens, food co-ops, community food centres);
 - iv) Food-related *home occupations*;
 - v) Small and medium scaled food retailers; and,
 - vi) Local market space (i.e. a farmer's market).

- n) Community gardens are encouraged in proximity to multi-unit residential housing and *special needs housing*, and may be located either on public or private land.

Source Water Protection

- o) Source water protection shall be regulated in accordance with the policies of Section 5.5 of the Georgina Official Plan. References to the schedules in Section 5.5 of the Official Plan shall be interpreted to instead reference Schedule D of this Plan for lands within the Keswick Secondary Plan Area.

Natural Hazards

- p) Natural Hazards shall be regulated in accordance with the policies of Section 4.4 of the Georgina Official Plan. References to Schedule C in Section 4.4 of the Official Plan shall not be applied and instead, detailed mapping provided by the Lake Simcoe Region Conservation Authority shall be utilized to determine the precise location of natural hazards within the Keswick Secondary Plan Area.

13.1.5 GENERAL LAND USE POLICIES

13.1.5.1 Land Uses Permitted in all Designations

- a) Land uses permitted in all designations as set out in Schedule B shall be regulated in accordance with the policies of Section 4.1 of the Georgina Official Plan.

13.1.5.2 Land Uses Prohibited in All Designations

- a) Land uses prohibited in all designations as set out in Schedule B shall be regulated in accordance with the policies of Section 4.2 of the Georgina Official Plan.

13.1.5.3 Residential Land Use Policies

13.1.5.3.1 Low-Rise Residential Uses

- a) Low-rise residential uses shall include single detached, semi-detached, duplex, tri-plex, *townhouses*, and low-rise apartment buildings with four or more units.
- b) Low-rise residential buildings shall have a maximum height of 3-storeys or 11 metres, whichever is less. In certain situations, and subject to the policies for *compatible development*, Council may permit additional height above 11 metres for a 3-storey low-rise residential building.
- c) Low-rise residential *development* shall have a net density of between 25 to 40 units per *net residential hectare*.
- d) While it is recognized that this Secondary Plan identifies a maximum density, such density shall only be permitted where it is considered *compatible* with existing development, to the satisfaction of the Town.

13.1.5.3.2 Mid-Rise Residential Uses

- a) Mid-rise residential uses shall include all forms of residential buildings containing four or more dwelling units such *live-work units*, *townhouses*, and mid-rise apartment buildings.
- b) Mid-rise residential buildings shall have a maximum height of 6-storeys or 20 metres, whichever is less.
- c) Mid-rise residential *development* shall have a net density of between 40 to 85 units per *net residential hectare*.
- d) While it is recognized that this Secondary Plan identifies a maximum building height and density, such building height and density shall only be permitted where they are considered *compatible* with existing development, to the satisfaction of the Town.
- e) New mid-rise residential *development* may be permitted subject to the following conditions:
 - i) The proposed *development* is *compatible*, and can be sensitively integrated with the surrounding land uses. Special measures such as *angular plane*, increased building setbacks, or enhanced landscaped buffer strips may be required in order to ensure sensitive integration;
 - ii) The site is adequate in size and configuration to accommodate on-site parking facilities and amenities;
 - iii) The transportation, utilities and service infrastructure can adequately serve the proposed *development*; and,
 - iv) The community and neighbourhood amenities such as parks, open space, recreational facilities and institutional services can adequately serve the proposed *development*.

13.1.5.3.3 High-Rise Residential Uses

- a) High-rise residential uses shall include apartment buildings.
- b) High-rise residential buildings shall be a minimum of 7-storeys and a maximum of 12-storeys or 38 metres, whichever is less.
- c) High-rise residential *development* shall have a net density of between 85 to 155 units per *net residential hectare*.
- d) New high-rise residential *development* may be permitted subject to the following conditions:
 - i) The proposed *development* is *compatible*, and can be sensitively integrated with the surrounding land uses. Special measures such as *angular plane*, increased building setbacks, or enhanced landscaped buffer strips may be required in order to ensure sensitive integration;
 - ii) The site is adequate in size and configuration to accommodate on-site parking facilities and amenities;

- iii) Transportation, utilities and service infrastructure can adequately serve the proposed *development*; and,
- iv) Community and neighbourhood amenities such as parks, open space, recreational facilities and institutional services can adequately serve the proposed *development*.

13.1.5.3.4 Additional Residential Units

- a) A maximum of two *additional residential units* (i.e. accessory apartments) may be permitted on a lot which contains a single detached, semi-detached or street or block townhouse dwelling and an associated detached building. Specifically, one *additional residential unit* may be permitted within each of the primary residential building and a detached building, subject to the policies of this Section.
- b) *Additional residential units* shall not be counted toward the net density maximums identified in this Secondary Plan.

Additional Residential Unit within the Primary Residential Dwelling

- c) An *additional residential unit* that is located within a single detached, semi-detached, or street or block townhouse dwelling is permitted subject to conformity with the provisions of the Zoning By-law and the Town's Accessory Apartment Registration By-law.

Additional Residential Unit within a Detached Building

- d) An *additional residential unit* is permitted in a detached building located on a lot that contains a primary single detached, semi-detached, or street or block townhouse dwelling.
- e) At the time of the Zoning By-law Comprehensive Review, the Town will further regulate an *additional residential unit* in a detached building.
- f) Prior to the Zoning By-law Comprehensive Review, the Town will consider Minor Variance Applications to permit an *additional residential unit* in a detached building. The following will be considered when reviewing applications to permit an *additional residential unit* in a detached building:
 - i) The parking requirements can be accommodated on the property, which may include tandem parking;
 - ii) The property fronts on and has access to an assumed municipal road. For properties that are part of a condominium development and front on a private road, prior to an *additional residential unit* being permitted, the Town shall be satisfied that there are appropriate provisions in place for year-round maintenance of the private road to support adequate ingress and egress of emergency vehicles; and,
 - iii) The *additional residential unit* shall have a maximum *gross floor area* of no more than 40% of the primary residential dwelling's *gross floor area*.

Garden Suites

- g) A *garden suite* may be permitted on a temporary basis on a lot that contains a single detached dwelling, subject to a Zoning By-law Amendment and in accordance with the following provisions:
- i) The lot is located within the Existing Neighbourhood designation;
 - ii) Not more than one *garden suite* may be permitted on a lot containing an existing single detached dwelling that has frontage on an assumed municipal road;
 - iii) The *garden suite* shall be designed as a portable unit that is subordinate in size and scale to that of the principal dwelling on the lot;
 - iv) Municipal sanitary sewer and water supply allocation is available to service the *garden suite*, or in areas not to be serviced by municipal sanitary sewer and water supply, that the *garden suite* can be adequately serviced by a private on-site septic system and private potable water supply;
 - v) The lot shall be of adequate size and shape to accommodate the *garden suite* so as to not create compatibility problems with surrounding properties. In this regard, the following shall be taken into consideration when considering a Zoning By-law Amendment for a proposed *garden suite*:
 - *Garden suites* shall be located in the rear yard and provide adequate setbacks from the lot lines;
 - Adequate amenity area exists for both the principal dwelling and *garden suite*; and,
 - A minimum of one parking space shall be provided for the *garden suite*.
 - vi) As a condition to the passing of a by-law authorizing the temporary use of a garden suite, Council may require the owner of the suite and/or subject property to enter into an agreement with the Town dealing with such matters related to the temporary use of the *garden suite*, including:
 - The installation, maintenance and removal of the *garden suite*;
 - The period of occupancy of the *garden suite* by any of the persons named in the agreement which shall not exceed 20 years from the day of the passing of the by-law. As permitted under the Planning Act, Council may by by-law, grant further extensions of not more than three years during which the temporary use is authorized; and,
 - The monetary or other form of security that Council may require for actual or potential costs to the Town associated with the *garden suite*.

13.1.5.3.5 Special Needs Housing

- a) *Special needs housing* includes all forms of communal housing including: *group homes, lodging houses, halfway houses, homes for special care and senior care facilities*. *Special needs housing* also includes all other types of residences licensed or funded under a

federal or provincial statute for the accommodation of persons living under supervision in a single housekeeping unit and who, by reason of their age, emotional, mental, social or physical condition, require a group living arrangement for their well-being.

- b) *Special needs housing* that is not a *halfway house* and accommodates 8 or less occupants (not including staff) shall be permitted in all designations that permit residential uses, subject to the satisfaction of the Town that any changes to a building resulting from the conversion to *special needs housing* shall be in keeping with the physical form and character of the surrounding neighbourhood.
- c) *Halfway houses*, and other forms of *special needs housing* that accommodate more than 8 occupants (not including staff), shall be permitted in all designations that permit residential uses, subject to the satisfaction of the criteria above for *special needs housing* and the following additional criteria:
 - i) The site is encouraged to be in proximity to transit and/or with direct access to a Collector or Regional Road;
 - ii) The site is located with convenient access to *public service facilities*; and,
 - iii) The lot size and configuration are sufficient to accommodate the building, required parking, green space and amenity areas.

13.1.5.3.6 Home Occupations

- a) A *home occupation* is a small-scale professional or personal service operation conducted by someone at his or her primary place of residence.
- b) *Home occupations* are typically conducted within a dwelling or dwelling unit, are permitted within a number of land use designations, and are further regulated by the Zoning By-law.
- c) A *home occupation* may also be permitted within an *accessory building or structure* by way of an approved Minor Variance, subject to the following criteria:
 - i) The use is carried out entirely within a dwelling unit or an *accessory building or structure*, or a combination thereof, and can be appropriately accommodated within said building(s) or structure(s);
 - ii) The use is clearly secondary to the primary use of the property as a residence in terms of floor space utilization;
 - iii) The residence is the principal residence of the person carrying out the *home occupation* use;
 - iv) The activities associated with the *home occupation*, including traffic generation, parking, noise and odour do not adversely affect the surrounding area;
 - v) Adequate water supply and sewage disposal facilities are available and the requirements of the Ontario Building Code are satisfied;
 - vi) Solid waste beyond the volume normally generated by a household and collected by the municipal waste collection program shall not be permitted;

- vii) No outside storage of goods, materials, equipment or service vehicles related to the *home occupation* use shall be permitted, except as permitted in the Zoning By-law;
- viii) Provision of adequate on-site parking; and,
- ix) The retail sales of any goods or wares shall not be permitted as a primary *home occupation* use.

13.1.5.3.7 Live-Work Units

- a) *Live-work units* integrate small-scale service commercial, retail or office uses at-grade while providing residential uses above.
- b) *Live-work units* may be permitted in low-rise and mid-rise residential buildings and shall provide:
 - i) Amenity areas which may be provided at grade or incorporated as a balcony or amenity space above a garage or on a rooftop;
 - ii) Buffering from adjacent residential dwellings with planting and/or fencing; and,
 - iii) Adequate parking and drop-off/pick-up facilities.

13.1.5.3.8 Short-Term Rental Accommodation

- a) A *short-term rental accommodation* may be permitted in association with a permitted single detached dwelling or an *additional residential unit* on the same lot as a single detached dwelling.
- b) A *short-term rental accommodation* shall only be established and operated in accordance with the provisions of the Zoning By-law and the Town's Short-Term Rental Accommodation Licensing By-law.

13.1.5.4 Public Service Facilities

Public service facilities include facilities designed to meet the recreational, social, health care, educational and cultural needs of residents. This include uses such as public libraries, recreation facilities, fire, police and other emergency services, schools, cultural centres or other similar uses, which are provided or subsidized by a government or other body.

- a) The Town will work with community organizations and other levels of government to ensure the provision of adequate *public service facilities* in appropriate locations within Keswick.
- b) In determining appropriate locations for *public service facilities*, the Town shall have regard for the type of service provided, recognizing that some uses will serve a localized population, while others will serve the whole or large portions of the Town, as well as tourists and other visitors.

- c) Where appropriate, *public service facilities* will be located in *community hubs* to promote cost-effectiveness and facilitate service integration, and access to transit and *active transportation*.
- d) *Public service facilities* shall have direct access from all parts of the surrounding community through a comprehensive *active transportation* network.

13.1.5.4.1 Emergency Services

- a) Fire, police, and other emergency service stations shall have convenient access to Regional Roads, a close relationship to the intended service area and shall be integrated with the surrounding uses, including appropriate architectural design, landscaping and buffering from residential buildings.
- b) *Development* shall be designed to support fire prevention and accommodate timely emergency response.

13.1.5.4.2 Education Facilities

- a) The Town will work with the School Boards to ensure the reservation of an adequate number and distribution of school sites and related facilities throughout Keswick to accommodate the needs of current and future residents. These sites and facilities shall be planned and developed in accordance with the respective policies, practices and guidelines of the School Boards.
- b) The Town shall encourage the location of school sites to be adjacent to parks or other recreation facilities to allow for shared use of facilities and shall work with the School Boards to allow public use of school facilities, under appropriate agreements or other satisfactory arrangements. The Town shall also encourage the development of shared school buildings where feasible and when the partnership criteria and policies can be met, to maximize the use of land and financial resources.
- c) The selection of school sites shall also require safe connectivity between the school site and the adjacent community, including direct sidewalk connections and the availability of community infrastructure that supports *active transportation* within the school catchment area.
- d) School sites shall be designed to prioritize the use of *active transportation* for trips to and from the school and traffic calming strategies may be considered along abutting roads to maximize safety.
- e) Schools are encouraged to be located near transit stops and/or near mixed-use areas, where appropriate. Secondary schools should be located within a 200-metre walking distance of a transit stop.
- f) *Existing* and proposed elementary and secondary school site locations are shown symbolically on Schedule B. In cooperation with the Town, a proposed school site location may be modified or relocated within a designation which permits *public service facilities*, without the need for an Amendment to this Secondary Plan.

13.1.6 LAND USE DESIGNATIONS

13.1.6.1 Local Strategic Growth Area

The *Local Strategic Growth Area* is one of the primary community structuring elements of Keswick from a land use and development perspective. The *Local Strategic Growth Area* consists of lands located along The Queensway Corridor and the Woodbine Avenue Corridor and within the three Urban Centres that are located along The Queensway Corridor.

It is the intent of this Secondary Plan to support and strengthen the *Local Strategic Growth Area* as locations for higher density, mixed-use development in a compact built form. It is expected that over time, the higher density built forms permitted within the *Local Strategic Growth Area* will broaden the mix and range of dwelling types and sizes within Keswick, thereby providing opportunities for more rental and *affordable housing options*.

Composition

- a) The *Local Strategic Growth Area* is identified on Schedule A and consists of the following land use designations as identified on Schedule B:
 - i) Mixed-Use Corridor 1 Designation – Woodbine Avenue;
 - ii) Mixed-Use Corridor 2 Designation – The Queensway; and,
 - iii) Urban Centres Designation:
 - Glenwoods Urban Centre;
 - Maskinonge Urban Centre; and,
 - Uptown Keswick Urban Centre.

Policies

- b) The portion of the *Local Strategic Growth Area* located within the *Delineated Built-up Area* shall be the focus of *intensification*.

13.1.6.1.1 Urban Centres Designation

Composition

- a) Schedule B designates the following three distinct Urban Centres along The Queensway Corridor: the Glenwoods Urban Centre located in the south end of Keswick; the Maskinonge Urban Centre located in central Keswick; and, the Uptown Keswick Urban Centre located in north Keswick. Each of these Urban Centres serve an important role in the community as follows:
 - i) **The Glenwoods Urban Centre** functions predominantly as a retail and service commercial centre combined with institutional and community uses and mid-rise residential development. The retail and service commercial role of this Urban Centre is expected to be expanded over time, primarily through *intensification* and *infill redevelopment*;

- ii) **The Maskinonge Urban Centre** functions predominantly as a tourist and recreational oriented area focused around marina related and other commercial uses surrounding the Maskinonge River. From the Maskinonge River the Urban Centre stretches north along The Queensway to the intersection of The Queensway, Metro Road North and Morton Avenue locally referred to as the "5-corners". It is envisioned that this Urban Centre will serve as a focal point of Keswick by developing into a mixed-use, tourist commercial, recreational and residential centre, with enhanced connections and public access to the Maskinonge River and Lake Simcoe shoreline; and,
- iii) **The Uptown Keswick Urban Centre** functions predominantly as a retail and service commercial area along The Queensway and Simcoe Avenue, surrounded by low-and mid-rise residential buildings and institutional uses. It is envisioned that this area will evolve into a centre containing a diverse array of specialized and boutique retail activities, restaurants, small-scale office and mid-rise mixed-use residential and commercial buildings.

Intent

- b) It is the intent of this Secondary Plan that the three Urban Centres are planned:
 - i) As destinations for residents, tourists and businesses;
 - ii) To promote economic revitalization, recognizing the potential for adaptive re-use, *redevelopment* and *intensification*;
 - iii) To be successful mixed-use neighbourhoods that accommodate a broad range of housing types and tenures, and contributes substantially to a complete, healthy and sustainable community;
 - iv) To include urban park spaces, where feasible, which connect to the Town's trail and *active transportation* network;
 - v) To act as hubs for local and regional transit, and to accommodate and support significant transit infrastructure; and,
 - vi) To provide *public service facilities*, amenities and infrastructure that will attract population and employment growth.

Permitted Uses

- c) Permitted uses on lands designated Urban Centres include:
 - i) *Existing* low-rise residential uses;
 - ii) Mid-rise residential uses;
 - iii) *Special needs housing*;
 - iv) *Home occupations*;
 - v) *Live-work units*;
 - vi) *Public service facilities*;

- vii) *Short-term rental accommodations;*
- viii) *Day care facilities;*
- ix) *Places of worship;*
- x) Private schools;
- xi) Restaurants, retail and service commercial uses, limited in size to 4,000 square metres of *gross floor area* per individual business;
- xii) Hotels;
- xiii) Business or professional office;
- xiv) Cultural, entertainment and recreational uses; and,
- xv) *Parking facilities* at-grade and/or in structures.

Policies

- d) Buildings within the Urban Centres designation shall have a minimum height of 2-storeys and a maximum height of 6-storeys or 20 metres, whichever is less. Where a minimum height of 2-storeys is not feasible or desirable for the site, Council may consider 1-storey buildings without an Amendment to this Plan. Further, additions or renovations to existing 1-storey buildings are not required to have minimum height of 2-storeys.
- e) The Urban Centres designation shall accommodate a mixture of uses distributed throughout each of the Urban Centres. A mixture of uses is encouraged not just within the designation in general, but also on individual *development* sites.
- f) *Development* along The Queensway should prioritize incorporating a mixture of uses.
- g) The design of buildings in the Urban Centres designation shall enhance the quality of the pedestrian environment by:
 - i) Incorporating high-quality design including built form, architectural details, landscaping and signage;
 - ii) Requiring that buildings be located close to the street, and shall facilitate pedestrian activity and access;
 - iii) Requiring that all buildings abutting The Queensway include articulated façades and the use of quality materials;
 - iv) Encouraging *active uses* at-grade abutting The Queensway such as restaurants, retail and service commercial uses to animate the streetscape; and,
 - v) Residential dwelling units abutting The Queensway shall be located above the first floor, at-grade along the rear or side façades of a building, or fronting onto side streets.
- h) Where feasible, *development* along The Queensway is encouraged to involve the consolidation of existing lots to create a more viable *development* parcel.
- i) Drive-thru facilities shall not be permitted in the Uptown Keswick Urban Centre and the Maskinonge Urban Centre designations.

- j) All lands within the Urban Centres designation are identified as being within a *Community Improvement Project Area*. The Town may consider the preparation of a *Community Improvement Plan* in order to establish a more detailed plan for the area, identify public realm improvement priorities, and establish incentive programs to assist the private sector in improving their properties.

13.1.6.1.2 Mixed-Use Corridor 1 Designation – The Queensway

Intent

- a) It is the intent of this Secondary Plan that the lands within the Mixed-Use Corridor 1 designation intensify over time with a mid-rise, mixed-use character, in a manner that is sensitive to the adjacent existing neighbourhood areas.

Permitted Uses

- b) Permitted uses on lands designated Mixed-Use Corridor 1 include:
 - i) Low-rise residential uses and mid-rise residential uses;
 - ii) *Special needs housing*;
 - iii) *Home occupations*;
 - iv) *Live-work units*;
 - v) *Public service facilities*;
 - vi) *Short-term rental accommodations*;
 - vii) *Day care facilities*;
 - viii) *Places of worship*;
 - ix) Private schools;
 - x) Business or professional office;
 - xi) Restaurant, retail and service commercial uses, limited in size to 1,000 square metres of *gross floor area* per individual business;
 - xii) Hotels;
 - xiii) Cultural, entertainment and recreational uses; and,
 - xiv) *Parking facilities* at-grade and/or in structures.

Policies

- c) Buildings within the Mixed-Use Corridor 1 designation shall have a minimum height of 2-storeys and a maximum height of 6-storeys or 20 metres, whichever is less. Where a minimum height of 2-storeys is not feasible or desirable for the site, Council may consider 1-storey buildings without an Amendment to this Plan. Further, additions or renovations to existing 1-storey buildings are not required to have minimum height of 2-storeys.

- d) The Mixed-Use Corridor 1 designation shall accommodate a mixture of uses along The Queensway Corridor. A mixture of uses is encouraged not just within the designation in general, but also on individual *development* sites.
- e) Low-rise residential uses may be permitted within the Mixed-Use Corridor 1 designation, only where the site abuts existing single detached or semi-detached dwellings, and the low-rise residential built form is considered necessary to provide an appropriate transition to the adjacent neighbourhood. However, the portion of the property that abuts The Queensway shall require *development* that is consistent with the mixed-use, mid-rise vision for The Queensway.
- f) Drive-thru facilities shall not be permitted in the Mixed-Use Corridor 1 designation.
- g) The design of buildings in the Mixed-Use Corridor 1 designation shall enhance the quality of the pedestrian environment by:
 - i) Incorporating high-quality design including built form, architectural details, landscaping and signage;
 - ii) Requiring that buildings be located close to the street, and shall facilitate pedestrian activity and access;
 - iii) Requiring that all buildings abutting The Queensway include articulated façades and the use of quality materials; and,
 - iv) Mixed-use and non-residential buildings abutting The Queensway shall include *active uses* at-grade that animate the streetscape.

13.1.6.1.3 Mixed-Use Corridor 2 Designation – Woodbine Avenue

Intent

- a) It is the intent of this Secondary Plan that lands within the Mixed-Use Corridor 2 designation provide an array of restaurants, retail and service commercial opportunities and *public service facilities* to support the growing population of Keswick and the Town in general.
- b) It is also the intent of this Secondary Plan to introduce mid-rise and high-rise residential uses into the Mixed-Use Corridor 2 designation through mixed-use developments to both support the ongoing urban evolution of Woodbine Avenue, and to provide opportunities for high-rise residential built forms in a location that is *compatible* with the existing community.

Permitted Uses

- c) Permitted uses on lands designated Mixed-Use Corridor 2 include:
 - i) Mid-rise residential uses, save and except for *ground-oriented* built forms;
 - ii) High-rise residential uses;
 - iii) *Special needs housing*;
 - iv) *Home occupations*;

- v) *Live-work units*, but only in the form of a mid-rise residential use;
- vi) *Public service facilities*;
- vii) *Day care facilities*;
- viii) *Places of worship*;
- ix) Private schools;
- x) Restaurants, retail and service commercial uses of all types and scales, including major retail uses;
- xi) Business or professional office uses;
- xii) Hotels;
- xiii) Cultural, entertainment and recreational uses; and,
- xiv) *Parking facilities* at-grade and/or in structures.

Policies

- d) Buildings within the Mixed-Use Corridor 2 designation shall have a maximum height of 12-stores or 38 metres, whichever is less.
- e) The Mixed-Use Corridor 2 designation shall accommodate a mixture of uses along the Woodbine Avenue Corridor. A mixture of uses is required not just within the designation in general, but also on individual *development* sites.
- f) *Development* within the Mixed-Use Corridor 2 designation shall be comprehensively planned to cohesively integrate both residential and non-residential uses. A minimum of 50% of the *gross floor area* within the Mixed-Use Corridor 2 designation shall be devoted to non-residential uses. This requirement shall be measured on aggregate over lands under the same ownership and designated Mixed-Use Corridor 2. An appropriate mixture of residential and non-residential uses and their functional integration as an urban district shall be required through the use of easements, driveways, joint-use agreements and other mechanisms, to the satisfaction of the Town. For the purposes of this policy, long-term care homes and retirement homes are considered residential uses.
- g) *Development* proposals within the Mixed-Use Corridor 2 designation shall require the submission and approval of:
 - i) A comprehensive urban design development concept plan illustrating the proposed road layout and internal site circulation, land uses, densities and built form, building placement, and landscape and open space areas;
 - ii) A report providing a functional assessment of traffic impact and site servicing required for the proposed development; and,
 - iii) A land use summary indicating the *gross floor area* and percentage of land dedicated to each land use type, the anticipated population, residential density, and number of jobs, and a summary of how the proposed *development* contributes toward the minimum 50% *gross floor area* requirement for non-residential uses within the Mixed-Use Corridor 2 designation as per 13.1.6.1.3(f).

- h) *Development* should include a physically cohesive group of businesses and mixed-use buildings designed in an integrated fashion with common internal traffic and pedestrian circulation.
- i) Direct vehicular access to Woodbine Avenue shall be discouraged, while shared access points with internal lanes connecting adjacent properties shall be required wherever possible.
- j) Associated storage, where permitted, shall not be visible from roadways. Storage shall be screened, buffered, landscaped, enclosed or interior to the site.
- k) New *development* shall be required to coordinate servicing, built form, access, land use, and *compatibility* with abutting lands. The scope and scale of the required coordination shall be determined by the Town, and may include the submission of a concept plan, as appropriate.
- l) The design of buildings in the Mixed-Use Corridor 2 designation shall enhance the quality of the pedestrian environment by:
 - i) Incorporating high-quality design including built form, architectural details, landscaping and signage;
 - ii) Requiring that buildings be located close to the street, and shall facilitate pedestrian activity and access;
 - iii) Requiring that all buildings abutting Woodbine Avenue include articulated façades and the use of quality materials; and,
 - iv) Mixed-use and non-residential buildings abutting Woodbine shall include *active uses* at-grade that animate the streetscape.

13.1.6.2 Neighbourhoods

Neighbourhoods within Keswick are envisioned to be predominantly a mix of low-rise and mid-rise residential areas, providing a range of *housing options* for current and future residents. The creation of neighbourhood focal points, which include neighbourhood supporting uses, are encouraged within Keswick's neighbourhoods. Neighbourhood focal points are intended to provide day-to-day services such as convenience retail, personal service shops, restaurants, elementary schools, and public parks. The proximity of these uses to one another will encourage and support *active transportation* and the concept of *complete communities*. Keswick's neighbourhoods are divided into Existing Neighbourhoods and New Neighbourhoods as identified on Schedule B.

13.1.6.2.1 Existing Neighbourhood Designation

Intent

- a) The Existing Neighbourhood designation applies to lands that are predominantly built-up with low-rise residential built forms and which were built as of April 27, 2020.

- b) Given the nature and character of the low-rise built form within the Existing Neighbourhood designation, these lands have limited potential to accommodate significant levels of *intensification*; however, they are prime candidates for the introduction of *additional residential units* and *home occupations*.
- c) The range of permitted non-residential uses within the Existing Neighbourhood designation are intended to serve and support the existing residential uses in the immediate vicinity without detracting from the primary residential character and function of the neighbourhood.

Permitted Uses

- d) Permitted uses on lands designated Existing Neighbourhood include:
 - i) Low-rise residential uses;
 - ii) *Additional residential units*;
 - iii) *Special needs housing*, with the exception of *halfway houses* and other forms of *special needs housing* that accommodate more than 8 occupants (not including staff);
 - iv) *Home occupations*;
 - v) *Short-term rental accommodations*;
 - vi) *Public service facilities*;
 - vii) *Day care facilities*; and,
 - viii) Neighbourhood supporting uses, in accordance with the policies of Section 13.1.6.2.3.
- e) In addition to the permitted uses listed above, *existing* local commercial uses such as convenience retail and personal service shops, along with *places of worship* and institutional and community uses are also permitted within the Existing Neighbourhood designation.
- f) Residential *development* as permitted in site-specific Official Plan Amendments or draft plans of subdivision approved prior to the adoption of this Secondary Plan are permitted within the Existing Neighbourhood designation.

Policies

- g) A mix of single detached, semi-detached and duplex dwelling units shall be distributed throughout the Existing Neighbourhood designation.
- h) *Townhouses*, tri-plexes, four-plexes, low-rise apartment buildings and *live-work units* may also be permitted on a limited basis within the Existing Neighbourhood designation where they are *compatible* with existing land uses and located adjacent a neighbourhood focal point such as a park, school, *place of worship* or neighbourhood supporting use.
- i) Buildings within the Existing Neighbourhood designation shall have a maximum height of 3-stores or 11 metres, whichever is less.

13.1.6.2.2 New Neighbourhood Designation

Intent

- a) The New Neighbourhood designation applies to lands that are primarily located within the *Designated Greenfield Area* and were vacant as of April 27, 2020.
- b) Lands within the New Neighbourhood designation are intended to accommodate a wide range of low- and mid-rise residential uses to diversify the housing supply in Keswick.

Permitted Uses

- c) Permitted uses on lands designated New Neighbourhood include:
 - i) Low-rise residential uses and mid-rise residential uses;
 - ii) *Additional residential units*;
 - iii) *Special needs housing*;
 - iv) *Home occupations*;
 - v) *Short-term rental accommodations*;
 - vi) *Public service facilities*;
 - vii) *Day care facilities*;
 - viii) *Places of worship*; and,
 - ix) Neighbourhood supporting uses, in accordance with the policies of Section 13.1.6.2.3.
- d) Residential *development* as permitted in site-specific Official Plan Amendments or draft plans of subdivision approved prior to the adoption of this Secondary Plan are permitted within the New Neighbourhood designation.

Policies

- e) A range and mix of low- and mid-rise residential uses shall be distributed throughout the New Neighbourhood designation.
- f) New *development* within this designation should be planned to achieve a minimum density of 50 persons and jobs combined per gross hectare.
- g) *Development* shall not exceed 4-storeys in height or 18 metres, whichever is less, save and except for low-rise and mid-rise residential uses which shall comply with Sections 13.1.5.3.1 and 13.1.5.3.2, respectively.

13.1.6.2.3 Neighbourhood Supporting Uses

- a) The Town may consider the integration of neighbourhood supporting uses at appropriate locations in the Existing Neighbourhood and New Neighbourhood designations through a Zoning By-law Amendment.
- b) Neighbourhood supporting uses shall be complementary to the primary residential function of neighbourhoods, serve the needs of residents in the community, and support the development of a walkable and *complete community*.
- c) Neighbourhood supporting uses shall be permitted as stand-alone buildings or integrated with residential uses in a mixed-use development.
- d) Neighbourhood supporting uses include:
 - i) Artisan studios and workshops;
 - ii) *Live-work units*;
 - iii) Business and financial offices, to a maximum of 200 square metres per use;
 - iv) Health care offices and clinics, to a maximum of 200 square metres per use;
 - v) Recreational and cultural facilities; and,
 - vi) Small-scale restaurants, convenience retail, personal service shops, to a maximum of 200 square metres per use.
- e) Any proposed neighbourhood supporting use will have regard for existing residential uses with respect to existing built form, building mass, scale, height, setbacks, orientation, landscaping, and visual impact.
- f) Where a proposed neighbourhood supporting use abuts a residential use, the Town shall require that impacts be mitigated through site design including appropriate screening and landscape treatments.
- g) Neighbourhood supporting uses and other non-residential uses such as *public service facilities* and parks and open space, should be clustered to create a neighbourhood focal point and to facilitate access by all modes of transportation.
- h) Neighbourhood supporting uses shall be located at the periphery of neighbourhoods and accessed from a Collector or Regional Road.
- i) To help establish neighbourhood focal points, neighbourhood supporting uses should locate together and be integrated with residential uses in mixed-use developments or in mixed-use buildings, where appropriate and desirable. In mixed-use buildings, neighbourhood supporting uses should be located on the ground floor, with residential uses located above.
- j) The Town will have regard for the following criteria when evaluating an application to permit neighbourhood supporting uses:
 - i) Whether the neighbourhood supporting use will contribute to a walkable and *complete community*;

- ii) Whether the site is suited to accommodate neighbourhood supporting uses due to matters such as noise, vibration, and other incompatibilities and *adverse effects*;
- iii) Whether the design, location and massing of the existing building or any new building, including lighting and signage, will be *compatible* with the surrounding residential uses;
- iv) Whether the neighbourhood supporting use is appropriately located at the periphery of a neighbourhood and has access to a Regional or Collector Road; and,
- v) Front yard parking is discouraged.

13.1.6.3 Community Supporting Uses

13.1.6.3.1 Tourist Commercial Designation

Intent

- a) It is the intent of this Secondary Plan to recognize and support the improvement and *development* of lands designated Tourist Commercial that are located along the Lake Simcoe shoreline.

Permitted Uses

- b) Permitted uses on lands designated Tourist Commercial include:
 - i) Restaurants, hotels, marinas, boat sales and rentals, and other similar tourist and recreational oriented commercial uses;
 - ii) Retail uses;
 - iii) Cultural, recreational and entertainment uses; and,
 - iv) *Parking facilities* at-grade and/or in structures.

Policies

- c) *Development* within the Tourist Commercial designation shall have a maximum height of 8-storeys or 26 metres, whichever is less.
- d) *Development* within the Tourist Commercial designation should prioritize providing and/or improving public access to Lake Simcoe where possible.
- e) All required parking for *development* within the Tourist Commercial designation shall be provided on-site.

13.1.6.3.2 Institutional/Community Designation

Intent

- a) It is the intent of this Secondary Plan that Keswick be well served by a full array of institutional and community uses to support the needs of residents. The Institutional/Community designation recognizes the major *existing* and planned public institutional and community uses.

Permitted Uses

- b) Permitted uses on lands designated Institutional/Community designation include:
- i) *Special needs housing;*
 - ii) *Public service facilities;*
 - iii) *Day care facilities;*
 - iv) *Places of worship;*
 - v) Cultural, recreational and entertainment uses;
 - vi) Government offices;
 - vii) Cemeteries;
 - viii) Private schools;
 - ix) Retail and service commercial uses ancillary to the other permitted uses, and limited in size to 400 square metres of *gross floor area* per individual business; and,
 - x) *Parking facilities at-grade and/or in structures.*

Policies

- c) Buildings within the Institutional/Community designation shall have a maximum height of 4-storeys or 18 metres, whichever is less.
- d) Ancillary retail and service commercial uses shall be located on the ground floor.
- e) Uses located within the Institutional/Community designation should be located within 500 metres of a public transit stop.

13.1.6.3.3 Day Care Facilities

- a) *Day care facilities* are permitted in accordance with the following policies:
- i) The use will not cause any traffic hazards or an unacceptable level of congestion on surrounding roads;
 - ii) The use is intended to serve and support the surrounding residential area;
 - iii) Where the *day care facilities* are for the care of children, they are in accordance with the Child Care and Early Years Act or any other relevant or successor legislation; and,
 - iv) Appropriately sized indoor facilities, on-site play areas, parking and drop-off facilities and appropriate buffering are provided, where required.

13.1.6.3.4 Places of Worship

- a) *Places of worship* may be permitted subject to the following criteria:
- i) The use is located on a Collector or Regional Road and will not cause any traffic hazards or an unacceptable level of congestion on surrounding roads;
 - ii) Adequate on-site parking, appropriate amenity areas and buffering are provided, where required; and,
 - iii) Direct access shall be provided to *places of worship* from all parts of the surrounding community through a comprehensive *active transportation* network.

13.1.6.4 The Natural Heritage System and Parks Network

A linked Natural Heritage System and Parks Network is a fundamental element of the urban structure of Keswick. The Natural Heritage System and Parks Network identified on Schedule A is comprised of lands designated Environmental Protection Area and Parks and Open Spaces on Schedule B.

13.1.6.4.1 Environmental Protection Area Designation

Composition

- a) The Environmental Protection Area designation is comprised of core conservation lands that predominantly contain *key natural heritage features* and *key hydrologic features*.
- b) The Environmental Protection Area designation also includes a 30-metre *vegetation protection zone* from the extent of identified *key natural heritage features* and *key hydrologic features*.
- c) Notwithstanding 13.1.6.4.1(b) and in recognition of *existing* residential development, the Environmental Protection Area designation that straddles the Maskinonge River does not include a 30-metre *vegetation protection zone*. Instead, the required 30-metre *vegetation protection zone* is incorporated into the Maskinonge River Overlay, which extends 120 metres from the extent of the Provincially Significant Maskinonge River Wetland Complex.

Intent

- d) The Environmental Protection Area designation is intended to:
 - i) Protect *key natural heritage features* and *key hydrologic features* and their ecological functions;
 - ii) Protect the health and water quality of Lake Simcoe and its tributaries;
 - iii) Conserve biodiversity; and,
 - iv) Protect surface and groundwater resources.

Permitted Uses

- e) Permitted uses on lands designated Environmental Protection Area include:
 - i) Forest, fish and wildlife management;
 - ii) Conservation, stewardship, restoration and remediation undertakings;
 - iii) Flood or erosion control projects, but only if they have been demonstrated to be necessary and in the public interest and after all alternatives have been considered;
 - iv) Passive recreational uses such as trails and bicycle paths;
 - v) An *existing* single detached dwelling and *accessory uses*, buildings or structures;
 - vi) A single detached dwelling and *accessory uses*, buildings or structures on an *existing* vacant lot of record, subject to the policies of this Section; and,
 - vii) *Home occupations*.

Policies

- f) Small-scale structures for recreational use, such as boardwalks, footbridges, fences, docks and picnic facilities may be permitted provided that the structures are designed and constructed to minimize the negative impacts on *key natural heritage features* and *key hydrologic features* and their ecological functions.
- g) Infrastructure works may be permitted, but only if the need for a project has been demonstrated through an Environmental Assessment or other similar environmental approval and there is no reasonable alternative. Where permitted, infrastructure design and construction shall be sensitive to the features and functions of the Environmental Protection Area designation, and include context sensitive design and innovative technologies to minimize impacts and enhance the features and function. Infrastructure within the Environmental Protection Area designation should avoid *key natural heritage features* and *key hydrologic features* and their *vegetation protection zones* where possible and shall be subject to the policies of applicable provincial plans.
- h) The boundaries and extent of the Environmental Protection Area designation identified on Schedule B are approximate. Minor refinements to these boundaries may occur through an Environmental Impact Study that demonstrates the appropriateness of the refinements to the satisfaction of the Town, in consultation with the relevant agencies. Such minor refinements will not require an Amendment to this Secondary Plan.
- i) Where the boundary to the Environmental Protection Area designation is refined, the abutting land use designation, or designations shall apply, provided the designation change will not result in *development* or *site alteration* that will have *adverse effects* on any *key natural heritage features* or *key hydrologic features* and their ecological functions.
- j) A 30-metre *vegetation protection zone* is required from the outset of all *key natural heritage features* and *key hydrologic features*. Notwithstanding the above, the required 30-metre *vegetation protection zone* may be increased or reduced based on the analysis and recommendations of an Environmental Impact Study approved by the Town, the Lake Simcoe Region Conservation Authority, and any other agency having jurisdiction.

- k) *Development* proposed on lands outside of the Environmental Protection Area designation containing *key natural heritage features* or *key hydrologic features* identified by subsequent study shall be subject to the Environmental Protection Area designation policies and permitted uses, including the submission of an Environmental Impact Study in accordance with the policies of this Secondary Plan.
- l) The removal, modification or destruction of *key natural heritage features* or *key hydrologic features* or their functions by unauthorized *development* or *site alteration* is prohibited. Such removal, modification or destruction will not provide the rationale for the removal of these lands from the Environmental Protection Area designation. Restoration, to the satisfaction of the Town in consultation with the Lake Simcoe Region Conservation Authority, will be required for these lands.
- m) An Environmental Impact Study shall be required prior to the establishment of any permitted use, demonstrating no *adverse effects* to *key natural heritage features* and *key hydrologic features* or their ecological functions.
- n) Where, through an application for *development* a *vegetation protection zone* is required to be established, the *vegetation protection zone* must be composed of native, non-cultivar, non-invasive species and maintained as natural self-sustaining vegetation.
- o) *Development* and *site alteration* is not permitted within the Environmental Protection Area designation, except in relation to the permitted uses and policies specified in this Section. Notwithstanding the above, *development* and *site alteration* shall not be permitted within:
 - i) *Wetlands*;
 - ii) *Habitat of endangered species, threatened species, and species of special concern*; and,
 - iii) *Fish habitat*, except in accordance with federal and provincial requirements.

To determine the presence of *habitat of endangered species, threatened species, and species of special concern*, the Town may require the submission of a Habitat Screening Report with *development* applications. The requirement for and scope of the Habitat Screening Report shall be determined by the Town and any agency having jurisdiction.
- p) Notwithstanding any other policy in this Secondary Plan to the contrary, one new single detached dwelling and *accessory uses*, buildings or structures thereto, may be permitted on an *existing* vacant lot of record subject to obtaining any necessary planning approvals and an Environmental Impact Study that demonstrates to the satisfaction of the Town, in consultation with relevant agencies, that the proposed dwelling will not result in any *adverse effects* to *key natural heritage features* or *key hydrologic features* or their ecological function. Notwithstanding, a new single detached dwelling shall not be permitted within *wetlands* or in the *Habitat of endangered species, threatened species, and species of special concern*.
- q) Ecological offsetting and/or compensation shall be required when there is an unavoidable loss of *key natural heritage features* or *key hydrologic features* in accordance with the policies of the Town, York Region, and the Lake Simcoe Region Conservation Authority, as applicable. Through the development review process an Ecological Offsetting Strategy

will be required, as necessary, as part of an Environmental Impact Study or Natural Heritage Evaluation.

- r) An Ecological Offsetting Strategy is a plan that identifies the means to compensate for feature loss with the overall objective of net environmental gain. An Ecological Offsetting Strategy shall be prepared by a qualified professional including an ecologist, biologist, forester, or landscape architect to the satisfaction of the Town in consultation with the Lake Simcoe Region Conservation Authority. The Ecological Offsetting Strategy shall comply with offsetting requirements of the Town and the Lake Simcoe Region Conservation Authority. Detailed plans such as a Planting Plan, Landscaping Plan, and Edge Management Plan will be required as conditions of draft plan approval of a subdivision or condominium, or site plan control approval.
- s) Notwithstanding 13.1.6.4.1(o)(i) and 13.1.6.4.1(p), with respect to wetlands that are not provincially significant and through an Environmental Impact Study are considered to be degraded and/or have negligible local or regional ecological value, *development* may be permitted subject to compensation, replacement, or other satisfactory arrangements as approved by the Town, Lake Simcoe Region Conservation Authority, or any other applicable agency having jurisdiction.
- t) Where lands designated Environmental Protection Area are under private ownership, it shall not be construed as implying that such areas are free and open to the public, or that they will be purchased by the Town or other public agency.
- u) Lands within the Environmental Protection Area designation shall not be considered as part of any dedication for public park purposes as provided for in the Planning Act. Lands within the Environmental Protection Area designation may be dedicated to the Town or other public authority, subject to the approval of the Town, without cost.
- v) The planning, design and construction of permitted infrastructure projects within the Environmental Protection Area designation shall enhance the natural environment, including providing passive recreational amenities and environmental restoration where appropriate.

13.1.6.4.2 Environmental Overlays

Composition

- a) Schedule C identifies three Environmental Overlays:
 - i) **Lake Simcoe Shoreline Overlay** – The Lake Simcoe Shoreline Overlay applies to lands within 120 metres of the Lake Simcoe shoreline;
 - ii) **Maskinonge River Overlay** – The Maskinonge River Overlay applies to lands within 120 metres of the extent of the Provincially Significant Maskinonge River Wetland Complex; and,
 - iii) **Adjacent Lands Overlay** – The Adjacent Lands Overlay applies to lands within 90 metres of the extent of the associated Environmental Protection Area designation. The 90-metre Adjacent Lands Overlay combined with the 30-metre *vegetation protection zone* from identified *key natural heritage features* and *key*

hydrological features contained in the Environmental Protection Area designation, combine to create a 120-metre wide overlay from identified *key natural heritage features* and *key hydrological features*.

Intent

- b) The intent of the Environmental Overlays is to identify *adjacent lands* in accordance with provincial policy to trigger the requirement for the preparation of an Environmental Impact Study to support a *development* application.

Permitted Uses

- c) Permitted uses on lands within an Environmental Overlay include:
 - i) All uses permitted by the underlying land use designation identified on Schedule B, subject to approval of the required Environmental Impact Study demonstrating that there will be no *adverse effects* on the natural features or their ecological functions.

Policies

- c) The policies in this Section must be read in conjunction with the policies of the associated underlying land use designation that is identified on Schedule B.
- d) Prior to any lands within an Environmental Overlay being considered for *development*, an Environmental Impact Study is to be undertaken by the proponent in accordance with the requirements of the Town, the Lake Simcoe Region Conservation Authority, and any other agency having jurisdiction.
- e) Subject to the approval of the required Environmental Impact Study by the Town, the Lake Simcoe Region Conservation Authority, or any other agency having jurisdiction, the lands may be *developed* in accordance with the permitted uses and policies of the underlying land use designation identified on Schedule B.
- f) In the review of *development* applications on lands within the Lake Simcoe Overlay or the Maskinonge River Overlay, or on *adjacent lands* to tributaries of Lake Simcoe, the Town shall consider, in consultation with the appropriate agencies, the restoration of watercourses and shorelines to their natural state. In this regard, an application for *development* on the above noted lands shall comply with the following where applicable:
 - i) Increase or improve *fish habitat* in streams, lakes, and *wetlands*, and any adjacent riparian areas;
 - ii) Include landscaping and habitat restoration that increases the ability of native plants and animals to use valley lands or riparian areas as wildlife habitat and movement corridors;
 - iii) Seek to avoid, minimize and/or mitigate impacts associated with the quality and quantity of urban run-off into receiving streams, lakes and *wetlands*; and,
 - iv) Establish or increase the extent and width of a self-sustaining *vegetation protection zone* adjacent to Lake Simcoe, as required by the policies of this Plan.

- g) New buildings, structures and uses as well as *accessory uses, buildings or structures*, and expansions to *existing* buildings and structures, shall only be permitted in a *vegetation protection zone* along the Lake Simcoe shoreline if:
 - i) There is no alternative but to place the structure in this area and the area occupied by such structures is minimized;
 - ii) The ecological function of the *vegetation protection zone* is maintained; and,
 - iii) Pervious materials and designs are used to the extent feasible.
- h) *Development* proposed along the Lake Simcoe shoreline may require the preparation of a coastal engineering study by a coastal engineer. The coastal engineering study shall identify the coastal processes associated with Lake Simcoe such as wave uprush, wind setup, dynamic beaches, erosion, and high lake levels. Where *development* is permitted, the study must outline the proposed measures to mitigate shoreline hazards. The study shall be prepared to the satisfaction of the Town and the Lake Simcoe Region Conservation Authority.

13.1.6.4.3 Requirements for an Environmental Impact Study

- a) *Development* requiring the submission of an Environmental Impact Study in accordance with the policies of this Plan, shall address to the satisfaction of the Town, the Lake Simcoe Region Conservation Authority, and any other agency having jurisdiction, the following:
 - i) Demonstrate that there will be no *adverse effects on key natural heritage features, key hydrologic features* or their associated ecological functions;
 - ii) Connectivity within and between *key natural heritage features* and *key hydrologic features* will be maintained and, where possible, improved or restored to support the movement of native plants and animals across the landscape;
 - iii) The removal or destruction of natural features or functions not identified by this Plan have been considered and avoided, where possible. Such features or functions should be incorporated into the planning and design of the proposed use, where possible;
 - iv) Recommend planning, design and construction practices that will maintain and where feasible, improve or restore the health, diversity and size of the *key natural heritage features* or *key hydrologic features*; and,
 - v) Identify the extent of the developable land, restrictions on permitted land use, and a sufficient *vegetation protection zone* width to protect the natural features and their ecological functions.
- b) Prior to the preparation of an Environmental Impact Study, the proponent is required to consult with the Town, the Lake Simcoe Region Conservation Authority, and any other applicable agency having jurisdiction to define the terms of reference of the Environmental Impact Study. The specific requirements for an Environmental Impact Study shall be determined at the pre-consultation stage.

- c) Notwithstanding any other policy in this Secondary Plan to the contrary, where an application for *development* is of a minor nature, the Town in consultation with any applicable agency having jurisdiction, may scope or waive the requirement for such Environmental Impact Study.

13.1.6.4.4 Parks and Open Space Designation

Composition

- a) The Parks and Open Space designation is comprised of the following components:
 - i) *Existing* and planned public parks;
 - ii) *Existing* public open spaces and linkages; and,
 - iii) *Existing* private parks and open spaces, primarily in the form of smaller waterfront properties that are owned and maintained by private beach associations. These properties provide recreational opportunities and access to the Lake Simcoe shoreline for those residents in the surrounding neighbourhood who are members of the beach association.

Intent

- b) It is the intent of this Secondary Plan to provide an integrated system of public parks and other publicly accessible open space, trails, and pathways within the community for active and passive recreational purposes.
- c) It is the intent of this Plan to recognize and continue to permit *existing* private parks and open space.

Permitted Uses

- d) Permitted uses on lands designated Parks and Open Space include:
 - i) Public parks and open space as classified under 13.1.6.4.4(e), including associated active and passive recreational facilities and activities;
 - ii) *Existing* private parks and open space, including associated active and passive recreational facilities and activities;
 - iii) Open space linkages;
 - iv) Conservation, stewardship, restoration and remediation uses;
 - v) Accessory buildings and structures to any permitted use; and,
 - vi) Limited retail commercial uses in public parks and open space areas which serve the main permitted use, such as concession stands, subject to all other policies of this Secondary Plan.
- e) Public parks and open space include:
 - i) **Community Parks** – are intended to serve the broader community area or a series of neighbourhoods and will be designed to offer senior-level athletic facilities and

more recreation opportunities than Neighbourhood Parks. Community Parks shall generally be between 4 to 8 hectares;

- ii) **Neighbourhood Parks** – are intended to serve local residents generally within walking distance and will be designed to offer active and passive recreation opportunities. Neighbourhood Parks shall generally be between 1.5 and 2 hectares, but not normally less than 1.5 hectares;
- iii) **Village Greens** – are intended to serve established higher density areas where the provision of a Neighbourhood Park is not practical or feasible. However, they are not intended to take the place of Neighbourhood Parks in planned future communities. Village Greens may be urban in nature, are typically less than 0.5 hectares, and should be planned as active use spaces that facilitate recreation for all ages; and,
- iv) **Open Space Areas** – are intended primarily for passive recreation uses. They may be developed, but should generally be publicly accessible. There is no minimum size recommended for public open space areas. The size, location and facilities will be determined by need and function.

Policies

- f) This Secondary Plan promotes healthy, active communities by planning and providing for a full range and equitable distribution of publicly accessible built and natural settings for recreation, including facilities, parks, open spaces, trails and linkages, and, where practical, water-based resources and woodlots.
- g) The Town will explore opportunities for increased public access to the Lake Simcoe shoreline through:
 - i) The securement of lands that abut the Lake Simcoe shoreline as those opportunities arise over time; and,
 - ii) The securement of waterfront parkland and waterfront trail linkages through the *development* approval process.
- h) *Development* within the Parks and Open Space designation shall be designed to enhance and complement the natural environment wherever possible, maintain the character of the landscape, and minimize disruption to adjacent existing land uses.
- i) New public parks shall be provided to the Town through parkland dedication as part of the *development* approvals process and in accordance with the provisions of the Planning Act. Lands dedicated to the Town for public parks and open space shall be in a location and condition acceptable to the Town.
- j) Wherever feasible, lands protected for environmental reasons shall be incorporated into the parks and open space system for leisure uses and compatible, passive recreational functions. Such environmentally protected lands shall not be included as part of the parkland dedication requirements of the Planning Act.
- k) The Town's Recreation Facility Needs Study, Parkland Standards Manual, and Parks Facilities and Construction Standard Detail shall provide further guidance on the location,

programming, design and construction of public parks, open spaces and recreational facilities.

- l) Not all lands that will ultimately be used for public parks and open space are designated Parks and Open Space on Schedule B. Public parks and open space, including associated active recreational uses and facilities are permitted in all land use designations save and except for the Environmental Protection Area designation. Within the Environmental Protection Area designation, only passive recreational uses and facilities are permitted in accordance with 13.1.6.4.1(e)(iv).
- m) Where a Parks and Open Space designation is applied to privately owned lands, the following policies shall apply:
 - i) It shall not imply that the lands are free and open to the general public; and,
 - ii) There is no obligation for the Town, or any other public agency, to purchase, improve or maintain the lands.

Policies for Open Space Linkages

- n) Open space linkages may be permitted in any land use designation and are not specifically identified on Schedule B. Open Space linkages may include utility corridors, or abandoned railway lines, all of which can contribute to a continuous linear open space system. Open space linkages shall not be counted toward any required parkland dedication.
- o) The Town may develop a system of multi-purpose trails within open space linkages for passive recreational purposes such as walking, jogging, cycling and mobility aid riding, and to provide access to *public service facilities*, in accordance with the following:
 - i) Where practical, the Town may acquire and develop natural linear features as open space linkages;
 - ii) This Secondary Plan encourages the use of utility corridors, rights-of-way and easements for open space linkages; and,
 - iii) Wherever possible, open space linkages shall be conveyed to the Town or other public agencies.

13.1.6.5 Site-Specific Exceptions

The site-specific exceptions listed below have been carried forward from the Keswick Secondary Plan, 2004. The location of each site-specific exception is identified on Schedule F.

- a) Notwithstanding the policies of the Tourist Commercial designation, on those lands described as Part Lot 17, Concession 2 (NG), and shown on Schedule F, an exclusive medium density residential development shall not be permitted. However, the redevelopment of the existing tourist commercial site for low density single detached dwellings may be permitted without Amendment to this Secondary Plan.

- b) On those lands described as Blocks 53, 54 and 55, Plan 65M-3332 and Part of Lot 7, Concession 3 (NG), and shown on Schedule F, a supermarket retail store or any other type of retail store, having as its primary purpose the sale of food and beverages (excluding alcoholic beverages), shall have a maximum floor area of 465 m². Furthermore, the floor area devoted to the sale of food and beverages (excluding alcoholic beverages) in any other types of retail store shall not exceed 465m². Further, vehicular access to the subject lands from Woodbine Avenue shall be restricted to one access.
- c) In addition to the permitted uses of the Existing Neighbourhood and New Neighbourhood designations, on those lands located within Development Area 4, being the South Keswick Development Area, as shown on Schedule F, medium density residential uses are also permitted.
- d) Notwithstanding the policies of this Secondary Plan, the approval of the Glenwoods Development Area Plan shall not be a prerequisite to the consideration and approval of applications for development within the Mixed-Use Corridor 2 designated lands within the Glenwoods Development Area Overlay. However, the Town shall still require studies demonstrating that the development of these lands will be coordinated with surrounding lands, in terms of various elements, including but not limited to, future road connections and servicing infrastructure.
- e) Notwithstanding the policies of the Urban Centres designation, on those lands described as Part of Lot 9, Concession 3, being Block 312 on Draft Approved Plan of Subdivision 19T-91002, and shown on Schedule F, only low and medium density residential uses and Special Needs Housing shall be permitted and the number of storeys per dwelling unit shall be limited to four.
- f) When preparing the design for development of those lands described as Part of Lot 7, Concession 3 (N.G.) also described as Block 114 on Plan 65M-3270, and shown on Schedule F, regard shall be given to ensure that the proposed lots are compatible with adjacent lot sizes and patterns. The proposed lots abutting existing residential lots shall have a minimum lot frontage of 15 metres.
- g) On those lands described as Lots 1 and 2, Registered Plan 103, and shown on Schedule F, an apartment building having a maximum of 7 dwelling units shall be permitted.

Amendment No. 113

- h) Notwithstanding the policies of this Secondary Plan, on those lands described as Part Lot 8, Lots 9, 10, 11 Registered Plan 225 and Parts 1, 2, 3, 65R19679 and Part Lot 13, Concession 3 (NG), and shown on Schedule F, an apartment building having a maximum of 97 dwelling units shall be permitted having no more than 2 bedrooms in each unit. Further, the maximum height of the apartment building to the top of the parapet of the sixth storey shall not exceed 18 metres and to the top of the architectural feature shall not exceed 28 metres.

Amendment No. 118

- i) Notwithstanding the policies of this Secondary Plan, on those lands described as Lots 104, 105 and 106 and Part of Block C, Registered Plan 238, and shown on Schedule F, a Medium Density Residential development having a maximum of 13 units and contained within 5 buildings with 2-4 units per building shall also be permitted. Further, the maximum

height of each building shall not exceed 2-storeys, and the residential density shall not exceed 23 units per net residential hectare.

Amendment No. 121

- j) Notwithstanding the policies of the Mixed- Use Corridor 2 designation, on those lands described as Part of Lot 9, Concession 3 (NG), Part 1, Reference Plan 65R-33362 and shown on Schedule F, only the following uses shall be permitted within the lands designated Mixed-Use Corridor 2:

- Business and profession offices;
- Retail and service commercial uses, excluding a restaurant; and,
- One residential unit within the expanded building.

Further, notwithstanding the policies of this Secondary Plan, an addition to the existing building, being approximately 7.3 m X 9.3 m (approximately 68 square metres) in area, connecting the existing building to the existing garage, shall be permitted within 15 metres of the top of the bank of the Maskinonge River provided it does not extend closer to the top of the bank than the existing buildings. Appropriate studies, as may be required, to support this addition, shall be provided at the time of site plan application.

Amendment No. 125

- k) On those lands described as Part Lot 15, Concession 3 (NG), and Part of the Former Road Allowance Between Concessions 2 and 3 (NG), being Parts 1 and 2, 65R-15088 and Part 1 65R-30415 and shown in Schedule F, the following provisions apply:

- i) Notwithstanding the policies of this Secondary Plan, development of land by consent, such that the Subject Land as identified on Schedule 'F' contains one existing dwelling on private services and accessory buildings and structures, and the Remainder Land also identified in Schedule F is vacant, shall be permitted.
- ii) The Town may, at its sole discretion, consider and possibly permit, a residential density transfer, up to a maximum of ten (10) dwelling units from the Subject Land to the Remainder Land, which is to be developed in conjunction with the adjoining property to the east as shown on Schedule F.
- iii) Development on the Remainder Land shall only proceed on municipal water and sewer services.
- iv) As part of the installation of municipal sanitary sewer and water mains to service the subdivision on the Remainder Land, the developer shall at its expense install sewer and water service lateral connections to the property line of the Subject Land. The benefitting property owner shall be responsible for, and shall bear the cost of, installing servicing pipes from the property line to their property. This requirement shall be included as a condition of approval of the draft plan of subdivision.
- v) A holding "H" provision by-law under the provisions of Section 36 of the Planning Act shall be utilized to implement items (i), (iii) and (iv) above. The terms under

which Council may remove the Holding "H" provision shall be stipulated in the amending zoning by-law.

- vi) The preparation of the Queensway West Development Area Plan as a prerequisite to subdivision approval on the Remainder Land and adjoining property to the east shall not be required provided these two parcels are developed comprehensively as one plan of subdivision.

Amendment No. 125

- l) On those lands described as Part of Lot 16, Concession 3 (NG), as shown in Schedule F, the following shall apply:

- i) The preparation of the Queensway West Development Area Plan as a prerequisite to subdivision approval shall not be required provided the lands south of Old Homestead Road are developed comprehensively as one plan of subdivision.

Amendment No. 128

- m) Notwithstanding the policies of this Secondary Plan, on those lands described as Part of Lots 2 and 3, Concession 3 (NG), and shown on Schedule F, semi-detached dwelling units are permitted as a medium density housing form, and a maximum residential density for the subject land shall not exceed 16.8 units per gross residential hectare.

Amendment No. 135

- n) Part Lots 6 & 7, Concession 3 (N.G.)

- a) Notwithstanding the policies of this Secondary Plan, the preparation of a development area plan for Development Area 3 – Glenwoods, as it applies to the Subject Lands shown in Schedule A Location Map attached hereto, shall not be required as a prerequisite to approval of a development application on the Subject Lands;
 - b) Notwithstanding the policies of this Secondary Plan the maximum residential density permitted on the Subject Lands shall be 15 units per gross residential hectare; and,
 - c) A blue heron protection and monitoring area ("Heronry") has been identified on lands west of the Subject Lands and south of Dovedale Drive. A Heronry is defined within the Natural Heritage Reference Manual and Significant Wildlife Habitat Criteria Schedules for Ecoregion 6E.

The Council for the Town of Georgina and the Lake Simcoe Region Conservation Authority seek to protect the Heronry. Therefore, a draft plan of subdivision on the Subject Lands shall identify areas of impact by delineating setbacks to the Heronry as recommended by the Lake Simcoe Region Conservation Authority. Where, on lands proposed for development (i.e. residential lots), such development or site alteration may impact the Heronry these lands shall be shown as a block(s) on the draft plan and such blocks(s) shall be subject to a holding zone provision in the implementing Zoning By-law Amendment.

Keswick Secondary Plan

Further development or site alteration within these blocks shall require written confirmation from the Lake Simcoe Region Conservation Authority that the Heronry is no longer active or that it has been demonstrated that development within this zone will not harm any remaining habitat area. This will require a post-construction monitoring program for two breeding seasons.

Relating to Deferral Area 3

- o) On those lands described as Part Lot 16, Concession 2 (NG) and shown on Schedule F, a townhouse development having a maximum of 20 residential units shall be permitted.

Amendment No. 131

- p) Notwithstanding the policies of the Mixed-Use Corridor 1 designation, the only permitted uses on the subject property are low, medium and high density residential uses with a maximum of 6 dwelling units. Furthermore, the maximum residential density for the subject property shall not exceed 86 units per net residential hectare.

Amendment No. 142

- q) Notwithstanding the policies of this Secondary Plan, on lands described as Part Lot 11, Concession 3 (NG) and municipally addressed as 14434 Woodbine Avenue, Keswick, the following shall apply:
 - i) High density residential uses, retail uses and commercial service uses shall be permitted; and,
 - ii) The maximum building height shall be in accordance with Zoning By-law No. 500, as amended. Any additional roof projections shall also be in accordance with Zoning by-law No. 500, as amended.

Amendment No. 146

- r) Notwithstanding the policies of this Secondary Plan, on lands described as Part of Lot 1, Land Registrar's Compiled Plan 12004, also described as Blocks 249, 250 and 251, 65M-4659, and Parts of Lots 14 and 15 Concession 2 (NG), shown as hatched on Schedule '1' to OPA 146, the maximum Low Density Residential density, for low density residential uses and street townhouse dwelling units, shall not exceed a combined density of 16 units per gross residential hectare.

This increase in density on lands shown in a hatched pattern on Schedule '1' to OPA 146 shall be achieved by transferring the permissible density from lands shown as crosshatched to lands shown as hatched.

The maximum Low Density Residential density of lands shown in a crosshatched pattern on Schedule '1' to OPA 146 shall be zero, except that one dwelling unit may be permitted thereon.

Further, the density of the lands shown in a crosshatched pattern on Schedule '1' to OPA 146 shall be further addressed by the Town through a Secondary Plan review process.

Furthermore, street townhouses, consisting of a maximum 8 dwelling units per dwelling are permitted on lands shown as hatched on Schedule '1' to OPA 146.

13.1.7 PROVIDING SUSTAINABLE SERVICES AND INFRASTRUCTURE

13.1.7.1 The Transportation System

Composition

- a) The transportation system contains the road network, the *active transportation* system and the public transit system.

Intent

- b) That the transportation system within Keswick provides integrated *multimodal* transportation options that are safe, efficient, economical, and convenient, while respecting the natural heritage features and character of the community.

Objectives

- c) To provide a *multimodal* transportation system that:
 - i) Has regard for the environmental, social and aesthetic character and amenities of the community;
 - ii) Is compatible with and serves existing and future land use patterns; and,
 - iii) Encourages convenient movement within the community as well as providing linkages to external transportation systems within the Town, York Region and throughout the Province.
- d) To provide a highly interconnected, efficient and safe *active transportation* system that include features such as benches, waste receptacles, bicycle racks, crosswalks, lighting and shade.
- e) To improve access to public transit and transportation facilities for special user groups.

13.1.7.1.1 General Transportation Policies

- a) New *development* shall provide an interconnected network of roads with pedestrian supportive streetscapes that provide for ease of access, orientation and safety for all users.
- b) New facilities or major improvements to the existing transportation system will only occur where such improvements are consistent with the existing character and amenities of the community and comply with the requirements of the appropriate road authority.
- c) Major capacity improvements to the existing road system will only occur when the need exists and all reasonable traffic control options have been previously implemented.
- d) Transportation facilities will be planned, designed and constructed to minimize the effects of noise, fumes and vibration on existing and future residential *development*.

- e) New institutional, commercial and industrial *development* applications shall include a *transportation demand management* strategy that recommends measures to reduce single occupancy vehicle trips such as carpooling, bicycle facilities, employee transit passes and alternative work arrangements. The Town in consultation with the applicable road authority may waive or scope the terms of reference for a *transportation demand management* strategy in consideration of the anticipated scale and impact of the proposed *development*.
- f) New *development* may require the submission of a Traffic Impact Study. The Town in consultation with the applicable road authority will determine the need for a Traffic Impact Study including its scope.
- g) The Town shall examine the need for the inclusion of electrical vehicle charging stations in conjunction with new *development* and incorporate applicable requirements in the Zoning By-law and the Town's Development Design Criteria.

13.1.7.1.2 Road Network

Composition

- a) The functional road classification within Keswick includes Regional Roads, Collector Roads, Local Roads, and Private Roads.
- b) Regional Roads, Collector Roads and Local Roads are identified on Schedule E and are intended to provide the following functions:
 - i) **Regional Roads** – Regional Roads are primarily envisioned as providing routes for vehicles, pedestrians and cyclists through Keswick and across Georgina. Regional Roads are intended to accommodate large volumes of traffic and will generally have a right-of-way-width of up to 36 metres. Refer to the York Region Official Plan and York Region standards for specific right-of-way widths, policies and requirements relating to Regional Roads.
 - ii) **Collector Roads** – Collector Roads are intended to carry traffic between Regional Roads and Local Roads. Through traffic will be discouraged from using these roadways and limited access to properties abutting these roadways will be permitted. Collector Roads will generally have a right-of-way width between 23 metres and 30 metres. Sidewalks, with street trees where possible, are required on both sides of all Collector Roads. Bicycle routes are to be considered on all Collector Roads; and,
 - iii) **Local Roads** – Local Roads are designed to accommodate only low volumes of traffic at low speeds and generally only serve local area trips. Local Roads will generally have a right-of-way width between 18 metres and 20 metres, however this may be reduced in circumstances where adequate snow storage capacity can be provided within the right-of-way and/or alternative development standards have been approved by the Town. Sidewalks are required on one side of all Local Roads with the exception of cul-de-sacs serving fewer than 20 residential dwelling units.

Unbuilt, proposed Local Roads are also identified conceptually on Schedule E and include those roads which are planned but not yet built as of April 27, 2020. The

location of these roads is shown for information purpose and are subject to change through the *development* review process.

- c) Private Roads are those roads that provide access to development on private property and are not under the jurisdiction of the municipality. This includes lanes on private property serving existing development and condominium roads providing access to units within a condominium development.

Policies

- d) Schedule E, together with the policies of this Section, form the basis for the provision of roads within Keswick, including right-of-way widths, access controls and design policies. Notwithstanding, the Town is not obligated to construct proposed local roads identified on Schedule E within a specific time-period.
- e) The road network will be developed under the principle of *complete streets*. This requires that both the construction of new roads and the reconstruction and repair of existing roads include appropriate facilities for pedestrians, cyclists, public transit, and private vehicles.
- f) The road network shall be designed to provide views of the Natural Heritage System and Parks Network where possible, to assist in the creation of a sense of place.
- g) A grid pattern of Regional and Collector Roads that takes into account transportation needs, natural features and existing and proposed land uses, shall be used to provide the community with an efficient road network.
- h) Future Local Road patterns shall be responsive to the existing road network and pattern of development.
- i) Local Roads will be interconnected with multiple route choices to Regional and Collector Roads where public transit routes and retail and service commercial facilities are most commonly located.
- j) Cul-de-sacs and crescent roads shall not be utilized unless they are abutting the Natural Heritage System and Parks Network, or unless it has been demonstrated that there is no other alternative way to access the lands.
- k) Reverse lot frontages shall be avoided and not considered unless demonstrated to be the only alternative.
- l) All road improvements and new road projects shall be undertaken in accordance with the provisions of the Municipal Engineers Association's Municipal Class Environmental Assessment.
- m) Specific lane requirements shall be determined at the detailed design stage and will be based on traffic demand and planning considerations such as the preservation of mature trees and the overall effect upon the streetscape.
- n) Where determined to be appropriate by the Town, traffic calming will be achieved by using:
 - i) Pedestrian-priority roads, *woonerfs* or *home-zones*;

- ii) Road design that discourages vehicle speeding through right-of-way curvature, complimentary streetscape design, building proximity to the road and boulevard street tree planting;
 - iii) Traffic circles and roundabouts;
 - iv) Narrowed traffic lane widths; and/or,
 - v) Fewer number of traffic lanes.
- o) Traffic calming may also be achieved through the use of on street parking. On-street parking should be designed as follows:
- i) Parking should be provided on at least one side of all Local and Collector Roads; and,
 - ii) On-street parking areas may be demarcated with special pavement treatment in specific areas in order to distinguish the parking lane from the roadway.
- p) Where an existing road allowance is less than the prescribed width and additional widening is required, the Town may require that such widening be obtained through the *development* approval process in accordance with the Planning Act.
- q) In areas where the majority of the land along a road, or a significant portion of a road, is developed at the time that this Secondary Plan is adopted, then the standards for the right-of-way of the road, as specified by this Secondary Plan, shall act only as a guide. Further evaluations may be undertaken to determine a practical right-of-way width that can serve both vehicular and non-vehicular traffic requirements along the road while minimizing *adverse effects* on existing development.

General Design Policies

- r) The planning and design of all roads shall ensure the coordinated installation of roadways, utilities, sidewalks, bicycle lanes, streetscaping elements, lighting and tree planting. Priority shall be given to providing adequate space for the safe movement of pedestrians and cyclists, including their safe transfers on and off transit vehicles.
- s) The planning and design of roads within new development shall facilitate the efficient movement of vehicles and provision of municipal services including but not limited to emergency response, waste and recycling pick-up and school bus pick-up and drop-off.
- t) All utilities shall be located underground. Where components of utilities must be located above ground, they should be located where there is no conflict with street trees. Utility providers are encouraged to explore innovative methods of screening utilities with streetscape elements such as gateways features and lampposts.
- u) York Region and/or the Town shall consider the following as general design policies for all roads and the road network in general:
 - i) Provide a grid road pattern that is pedestrian and bicycle friendly, highly connected, and *transit-supportive*;

- ii) Ensure that the road pattern establishes development blocks that achieve an orderly pattern of *development* and visual diversity;
 - iii) Provide access for pedestrians, bicycles and vehicles, opportunities for vistas, view corridors, pedestrian amenity areas and space for utilities and services;
 - iv) Design all streetscape elements including plantings, trees, sidewalks, utility poles, paving patterns, bicycle racks, seating, natural or built shade structures, signage and waste/recycling receptacles to be consistent and complementary to the character of the community; and,
 - v) Design street lighting with regard for vehicular, cyclist and pedestrian requirements so that the size, height, and style of lighting reflect the hierarchy of the road and complement the character of the community.
- v) The design of all Town roads shall have regard for the Urban Design and Architectural Control Guidelines appended to the Secondary Plan and comply with Town's Development Design Criteria, Streetscape Design Criteria, or any other Council approved guidelines or programs as applicable.

Regional Roads and Collector Roads

- w) York Region and/or the Town shall consider the following design policies for Regional Roads and Collector Roads:
- i) Transit facilities including, but not limited to, transit shelters, lay-bys and dedicated transit lanes, may be located on any Regional Road or Collector Road;
 - ii) Individual direct access to any development site abutting a Regional or Collector Road shall be limited to minimize disruptions to traffic flow and to maximize safety and the attractiveness of the road;
 - iii) Buildings that abut any Regional or Collector Road shall present a façade with architectural detailing and landscape features that addresses the road frontage. Reverse frontage development shall not be permitted adjacent to any Regional or Collector Road; and,
 - iv) Road designs for any Regional Road or Collector Road are encouraged to include a planted centre median or other design features to signify their importance as a gateway or entrance feature.
- x) Regional Roads will generally limit access to commercial uses, industrial uses and existing lots. New residential access may be permitted only where:
- i) Traffic movement, volume, speed and safety are not compromised;
 - ii) Where no alternative Local or Collector Road access is avoidable; and,
 - iii) Where the entrance criteria of York Region are met.

Local Roads

- y) The Town shall consider the following design policies for Local Roads:
 - i) Individual direct access onto Local Roads is permitted subject to the Town's traffic by-law(s) and other municipal regulations governing Local Roads;
 - ii) Buildings that abut Local Roads shall present a façade with architectural detailing and landscape features that addresses the road frontage;
 - iii) Local Roads that are single loaded may include a reduced boulevard abutting a publicly owned stormwater management feature, open space, parkland or an environmental feature. Window roads fronting on a public road may also include a reduced boulevard; and,
 - iv) Transit routes and facilities may be permitted on Local Roads.
- z) In the context of this Secondary Plan, Lanes are a form of a municipally owned and maintained road. The Town shall consider the following as general design policies for Lanes:
 - i) Lanes provide access to private residential garages in rear yards, or access to the rear yard of a commercial building or site;
 - ii) Lanes should be provided where garages and front yard parking will detract from the character of a specific location, such as along a retail street, or where access may be restricted such as along Regional or Collector Roads; and,
 - iii) The use of permeable materials for Lanes shall be encouraged in areas where sufficient drainage exists.

Private Roads

- aa) The Town shall consider the following design policies for Private Roads:
 - i) Where appropriate, sidewalks may be required on one side of a Private Road serving a condominium development. Such sidewalk may be located within the utility corridor; and,
 - ii) The use of permeable materials for Private Roads shall be encouraged in areas where sufficient drainage exists.

Development on Assumed Public Roads and Private Roads

- bb) *Development* on assumed public roads and private roads shall be regulated in accordance with the policies of Section 9.2.2 of the Georgina Official Plan.

Realigned Glenwoods Avenue

- cc) It is intended that Glenwoods Avenue east of Woodbine Avenue be realigned to connect to the existing Glenwoods Avenue alignment west of Woodbine Avenue, as indicated on Schedule E. The realignment is expected to provide a key connection between Woodbine Avenue and the proposed future Highway 404 and Glenwoods Avenue interchange.

Transportation Demand Management

- dd) *Transportation demand management* measures to reduce single occupancy vehicle trips shall be identified in transportation studies submitted in support of *Major Development* applications.

13.1.7.1.3 Active Transportation System

Composition

- a) The *active transportation* system includes pedestrian friendly routes such as sidewalks, paved shoulders, bike lanes, multi-use paths, and trails.

Intent

- b) It is the intent of this Secondary Plan to provide a robust *active transportation* system and associated facilities to encourage *active transportation* within the community.
- c) The *active transportation* system is intended to play a crucial role in creating an integrated parks and open space system and support an overall *complete community*.

Policies

- d) New *development* shall contribute to the creation of a walkable and connected community with multiple destinations within walking distance of all residents.
- e) New *development* shall establish a connected and permeable grid pattern of roads with blocks that achieve an orderly and efficient development pattern that is pedestrian and bicycle friendly and supports public transit. The length of blocks shall be between 150 to 250 metres.
- f) Sidewalks are required on both sides of all Collector Roads and on one side of Local Roads, with the exception of cul-de-sacs serving fewer than 20 residential dwelling units.
- g) *Active transportation* routes such as bike lanes and multi-use paths shall connect to the road network, sidewalk network and community amenities at locations that are safe and convenient for pedestrians and cyclists.
- h) *Active transportation* routes such as sidewalks, bike lanes, trails, and multi-use paths shall include streetscaping elements that promote pedestrian and cyclist comfort and safety.
- i) *Active transportation* routes and facilities shall be designed in a manner that is sensitive to the environment and existing and proposed residential uses.
- j) *Development* proposals abutting *active transportation* routes shall be designed and buffered to mitigate any potential *adverse effects* between land uses.
- k) Where possible and feasible, unopened municipal road allowances, easements and utility corridors shall be utilized to enhance the trails system.
- l) The trails system includes trails within natural heritage features, stormwater management facilities, open spaces, parks and the road system. Trail design and type will be based on each site's individual characteristics in order to minimize environmental impacts.

- m) *Active transportation* routes and facilities will be designed to accommodate a range of users and abilities.
- n) Trails located in proximity to *key natural heritage features* and *key hydrologic features*, or adjacent to stormwater management facilities should incorporate interpretive signage.
- o) Trails and bicycle routes may be located within the right-of-way, but, where possible, should be separated from the travelled portion of the road by a landscaped buffer.
- p) *Active transportation* routes and facilities shall be designed and constructed in accordance with the Town's Trails and Active Transportation Master Plan, Regional Transportation Master Plan, and the Regional Pedestrian and Cycling Planning and Design Guidelines, as applicable.

13.1.7.1.4 Public Transit System

Intent

- a) It is the intent of this Secondary Plan to promote the use of public transit as an alternative to private vehicles. As such, public transit shall be a priority for reducing traffic, promoting *active transportation* and avoiding road expansions through established neighbourhoods. The public transit system shall be an integral part of the transportation network.

Policies

- b) The Town will work with York Region to develop and improve the public transit system in Keswick. Public transit services will be implemented through phasing based on meeting operational and financial criteria.
- c) *Major Development* applications shall be required to demonstrate how the proposed *development* is transit-oriented, in accordance with the York Region Transit-Oriented Development Guidelines.
- d) Within the Urban Centres and Mixed-Use Corridors, compact *development* at higher densities are preferred to maximize transit feasibility and accessibility.
- e) Areas that are to be developed or redeveloped shall be designed to incorporate land use and road patterns that support and permit convenient access to the public transit system.
- f) Building entrances, trails and the *active transportation* system shall be coordinated with the public transit system to maximize the accessibility and convenience of transit stops.

13.1.7.1.5 Parking Management

- a) All *development* shall include context appropriate on-site parking for vehicles and bicycles, as required in the Zoning By-law.
- b) Surface parking lots are permitted; however, parking may also be provided in parking structures, either above or below grade subject to site conditions and feasibility. Parking lots are subject to the following requirements:
 - i) *Parking facilities* at-grade and/or in structures shall be appropriately designed and landscaped to complement the urban streetscape;

- ii) Above grade parking structures shall, where possible, incorporate *active uses* at-grade facing onto any Regional or Collector Road;
 - iii) Surface parking lots shall not be located between the building and the road on The Queensway or Woodbine Avenue, except for where existing buildings or other factors or site conditions prevent all of the parking from being located in the rear or at the side of the building; and,
 - iv) Access to parking facilities shall be from Local Roads or from shared access points, wherever possible.
- c) The Town may enter into partnerships and/or develop a system for the provision of adequate off-street parking in defined parts of Keswick.
 - d) The development of shared parking spaces between *public service facilities* and public parks is encouraged where the facilities are within walking distance.
 - e) The Town shall consider providing bicycle parking facilities at all municipally owned and operated facilities to promote *active transportation*, where practical and feasible.
 - f) Bicycle parking facilities shall be considered for all new *development*, where practical.
 - g) The Town encourages opportunities to reduce the need for vehicular *parking facilities*, including reduced minimum parking space requirements in proximity to existing transit or a high concentration of local destinations, shared parking between complementary uses, on-street parking and preferential locations for carpooling and car-sharing spaces.

13.1.7.2 Sanitary Sewage and Water Supply Services

Intent

- a) The policies of this Section are intended to ensure that new growth and *development* takes place on full municipal sewer and water supply services in an orderly and cost-efficient manner.

Policies

- b) The Town supports the improvement and extension of municipal sanitary sewage and water supply services in accordance with the provisions of this Secondary Plan and other relevant municipal, regional and provincial policies and regulations, and within the financial capabilities of the Town and/or Region and on the basis of an approved capital budget program.
- c) All *development* within Keswick shall be serviced with municipal sanitary sewage and water supply.
- d) Planning for sewer and water services shall:
 - i) Promote the efficient use and optimization of existing infrastructure;
 - ii) Prepare for the impacts of a changing climate;
 - iii) Promote water conservation and water use efficiency;

- iv) Be coordinated and integrated with land use planning considerations; and,
 - v) Require design and construction practices that reduce inflow and infiltration into municipal sanitary infrastructure.
- e) The cost of providing full municipal services to facilitate the *development* of lands shall not impose a financial burden on existing taxpayers. Accordingly, such servicing costs shall be the responsibility of the proponent, with the ability for cost-sharing with future benefitting parties, through agreement with the Town.
- f) In extending services to develop specific vacant parcels, the proponent shall provide adjacent existing unserviced or partially serviced residences with the opportunity to connect to the municipal servicing system. It is expected that landowners will co-operate with each other in order to facilitate the *development* of lands on full municipal services, including permitting access to existing services by way of easements or in such a manner satisfactory to the Town.
- g) In areas where municipal sanitary sewage and/or water supply services are not yet available, existing uses, buildings and structures may be serviced by private sewage disposals and/or private well water supply. Further, the *development* of a single detached dwelling on an *existing* vacant lot of record may be permitted on private services, subject to any planning approvals and other policies of this Secondary Plan and the Lake Simcoe Protection Plan.
- h) The cost associated with extending municipal services to facilitate the connection of an existing building and/or developed lands in currently unserviced areas shall be the responsibility of the benefitting property owner(s).
- i) Property owners are encouraged to connect existing development to municipal services when they become available.
- j) Servicing capacity allocation in the Keswick Water Resource Recovery Facility and the size of trunk sewers shall be calculated on the basis of the factors assigned by the Town in consultation with York Region.
- k) *Development* shall be designed and constructed in accordance with a Functional Servicing Plan or Plans dealing with sewage and water systems. A Functional Servicing Plan shall be prepared in accordance with criteria established by the Town and shall be submitted prior to, or concurrently with *development* applications. Such Plan shall be prepared to provide for the continuous, orderly extension of services in a cost-effective manner.
- l) The Keswick Water Resource Recovery Facility is identified on Schedule B. Notwithstanding the land uses permitted by this Secondary Plan, in accordance with provincial guidelines, no *development* that includes *sensitive land uses* or other incompatible *development*, shall be permitted within 150 metres of the Keswick Water Resource Recovery Facility. However, the 150-metre setback may be reduced to a minimum of 100 metres, subject to the completion of an odour or similar study to be approved by the Town and York Region, which demonstrates that a lesser setback is appropriate.

Policies Respecting Municipal Sewage and Water Allocation

- m) The Town shall only approve *development* that can be allocated municipal servicing allocation, in conformity with Town and York Region policies, procedures and By-laws. The assignment of servicing allocation to *development* is at the sole discretion of the Town. Depending on the amount of servicing allocation available, not all *development* proposals may be able to proceed.
- n) The Town may choose to allocate municipal sewer and water servicing based on a servicing allocation assignment program.
- o) No dwelling, dwelling unit or other form of *development* requiring servicing allocation shall be constructed unless such land has been assigned the appropriate amount of sewage and water supply servicing allocation by Council.
- p) The Town shall assign municipal sewage and water servicing allocation for *development* requiring servicing allocation and draft plan of subdivision and/or draft plan of condominium approval according to the following procedures:
 - i) At the pre-consultation stage, Town Staff will advise the applicant if servicing capacity is currently available to facilitate the proposed *development*;
 - ii) Servicing allocation will only be set aside by Town Staff for the proposed *development* upon receipt of a complete Draft Plan of Subdivision and/or Draft Plan of Condominium Application(s);
 - iii) Servicing allocation will be preliminarily assigned to the proposed *development* at the time Council grants draft plan approval;
 - iv) Servicing allocation is considered to be formally assigned to the proposed *development* at the time of registration of the draft plan of subdivision and/or condominium; and,
 - v) If the draft plan of subdivision and/or condominium is not registered in the appropriate land registry office within three years from the date of draft plan approval, or the draft plan has not proceeded to the satisfaction of the Town within the term of draft plan approval, Council, at the time of considering an extension of the draft plan approval, may revoke the preliminary assignment of municipal servicing allocation, in whole or in part. Prior to revoking the allocation, the Town shall afford the proponent an opportunity to address Council on the matter. Servicing allocation shall be formally assigned at the execution of the Subdivision Agreement and/or Condominium Agreement.
- q) The Town shall assign municipal sewage and water servicing allocation for *development* requiring servicing allocation and site plan control approval according to the following procedures:
 - i) At the pre-consultation stage, Town Staff will advise the applicant if servicing capacity is currently available to facilitate the proposed *development*;
 - ii) Servicing allocation will only be set aside by Town Staff for the proposed *development* upon receipt of a complete Site Plan Control Application;

- iii) Servicing allocation will be preliminarily assigned to the proposed *development* by the Site Plan Review Committee, or Town Staff in the case of delegated approval authority, at the time the Site Plan Control Application receives approval;
 - iv) Servicing allocation is considered to be formally assigned to the proposed *development* at the time of the execution of a Site Plan Agreement, or upon satisfactory completion of all terms and conditions in the case of a Town Staff delegated approval; and,
 - v) At any stage prior to the issuance of a building permit for the proposed *development* or any portion thereof, Council may review the progress and status of the project. Where the project has not proceeded to the satisfaction of the Town after three years from the time of the submission of a complete Site Plan Control Application, Council may revoke the assignment of municipal servicing allocation, in whole or in part. Prior to revoking the allocation, the Town shall afford the proponent an opportunity to address Council on the matter.
- r) York Region shall be notified of any assignment of sewage and water allocation at the draft plan of subdivision or condominium approval or the site plan control approval stage.

13.1.7.3 Stormwater Management

Intent

- a) It is the intent of this Secondary Plan to manage impacts from stormwater on Lake Simcoe, the Maskinonge River and other watercourses within the watershed in order to maintain and enhance water quality, protect fish and wildlife habitat and prevent erosion.

Policies

- b) Stormwater management facilities shall be permitted in all land use designations. New stormwater management facilities located in the Environmental Protection Area designation shall be subject to the policies of the Environmental Protection Area designation, including the submission of an Environmental Impact Study to be approved by the Town in consultation with the Lake Simcoe Region Conservation Authority.
- c) All new *development* shall utilize stormwater management techniques to control the quality of run-off and control erosion and sedimentation during and after construction, in order to minimize *adverse effects* on the receiving body of water.
- d) Planning for stormwater management shall:
 - i) Be integrated with planning for sewage and water services and ensure that systems are optimized, feasible and financially viable;
 - ii) Minimize, or, where possible, prevent increases in contaminant loads;
 - iii) Minimize changes in water balance and erosion;
 - iv) Prepare for the impacts of a changing climate through the effective management of stormwater, including the use of *green infrastructure*;

- v) Mitigate risks to human health, safety, property and environment, including preventing the accumulation of standing water conducive to mosquito larvae breeding;
 - vi) Maximize the extent and function of vegetative and pervious surfaces; and,
 - vii) Promote stormwater management best practices, including stormwater attenuation and re-use, and *Low Impact Development*.
- e) Stormwater management facilities will be key features within the community contributing to a positive appearance and community image, while achieving functional objectives related to flow moderation and water quality.
- f) Stormwater management ponds will not be fenced, but rather where possible designed with trails, overlooks and interpretive signage so that they are an integral part of the parks and open space network. Where there is a need to discourage public access to areas around the perimeter of a stormwater management pond, living fences and barrier plantings will be utilized in place of fencing. Barrier plantings will be installed along the crest of steep slopes, adjacent to deep-water areas and around inlet and outlet structures.
- g) An application for *Major Development* shall be accompanied by a Stormwater Management Plan that:
- i) Applies best management practices to meet or exceed enhanced protection criteria (or equivalent) as outlined in the Ministry of the Environment, Conservation and Park's Stormwater Management Planning and Design Manual, as amended from time to time;
 - ii) Evaluates stormwater management on a "watershed" based approach and that is consistent with local subwatershed evaluations and water budgets, where available;
 - iii) Incorporates an integrated treatment train approach to minimize stormwater management flows and reliance on end-of-pipe controls through measures such as rainwater harvesting, permeable surfaces, clean water collection systems, run-off reduction of solids and materials at source and other source controls, lot-level controls and conveyance techniques, such as grass swales, constructed wetlands, bio-retention swales, green roofs, and the preservation and enhancement of native vegetation cover, where appropriate;
 - iv) Offers specific direction on how end-of-pipe stormwater management works shall be designed to satisfy at a minimum, the enhanced protection level specified in the Ministry of the Environment, Conservation and Park's Stormwater Management Planning and Design Manual, as amended;
 - v) Identifies the specific location of permanent end-of-pipe facilities, the areas they will service, and considerations for their size, shape and design criteria;
 - vi) Evaluates, at appropriate geographic scales, predicted changes in the water balance between *pre-development* and *post-development* conditions, and evaluates how such changes will be minimized;

- vii) Evaluates, at appropriate geographic scales, anticipated changes in phosphorus loadings between *pre-development* and *post-development*, and evaluates how phosphorus loading will be minimized;
 - viii) Identifies criteria and circumstances upon which interim stormwater management facilities may be considered or precluded;
 - ix) Demonstrates consistency with the Lake Simcoe Protection Plan and stormwater management master plans prepared by the Town and/or the Lake Simcoe Region Conservation Authority, as applicable; and,
 - x) Notwithstanding the policies above, where an application for *Major Development* is of a minor nature, the Town in consultation with any relevant agency, may waive the requirement to conduct a Stormwater Management Plan or scope down the study requirements.
- h) The Stormwater Management Plan must address water quantity controls to limit the *post-development* flows to *pre-development* flows for the 1 in 2-year to the 1 in 100-year storm events. A minimum 24-hour detention of run-off from a 25 mm storm shall be required for erosion protection and base flow maintenance, where feasible.
- i) Stormwater management facilities are to be located above the existing 1 in 100-year floodplain; however, incremental storage between the 1 in 100-year and regulatory floodline is to be maintained. The volume below the pond's high-water level shall not be considered as available storage for the regulatory floodplain. Berming for such facilities within the floodplain shall not exceed a 0.3-metre elevation higher than the existing ground elevation.
- j) Every owner and operator of a new stormwater management facility shall be required to inspect and maintain the works on a periodic basis. Furthermore, every owner and operator of a new *priority stormwater management works* shall be required to monitor the operation of the works, including monitoring the quality of the effluent from the works, on a periodic basis as determined by the Town and any other agency having jurisdiction and as stipulated in a Development Agreement.
- k) An application for *Major Development* shall be accompanied by a Preliminary Phosphorus Budget as part of an overall Functional Servicing Report or Preliminary Stormwater Management Report. This evaluation shall be prepared by a qualified professional to the satisfaction of the Town, in consultation with the Lake Simcoe Region Conservation Authority, prior to any draft plan of subdivision or condominium approval, site plan control approval, or granting of a provisional consent approval. A detailed Phosphorus Budget, based on the approved Preliminary Report, will be required as a condition of draft plan of subdivision or condominium approval, site plan control approval, or of provisional consent approval.
- l) The Phosphorus Budget must demonstrate that the phosphorus load from the proposed *development* will be reduced to zero. The Phosphorus Budget shall be prepared in accordance with the following:
- i) Subwatershed Evaluations under 8.3-SA of the Lake Simcoe Protection Plan;
 - ii) Designated Policy 4.8 of the Lake Simcoe Protection Plan;

- iii) Section 2.2 of the Provincial Policy Statement;
 - iv) Lake Simcoe Region Conservation Authority's Technical Guidelines for Stormwater Management Submissions;
 - v) Provisions and Regulations of the Ontario Water Resources Act; and,
 - vi) Policy 3.2.7 of the Growth Plan for the Greater Golden Horseshoe.
- m) In those situations where the requirement for a phosphorus load of zero cannot be met or demonstrated in a post-*development* scenario, the developer or proponent shall be required to provide phosphorus offsetting to the Lake Simcoe Region Conservation Authority, in accordance with Lake Simcoe Region Conservation Authority Phosphorus Offsetting Policy. Phosphorus offsetting shall generally occur in the same catchment as the subject lands, subwatershed, municipality, or watershed in order of priority.
- n) Proper agreements shall be established in order to ensure that the phosphorus offset will be employed and maintained in perpetuity. The following agreements or legal instruments, where appropriate, shall be required as a condition of approval for any draft plan of subdivision or plan of condominium, site plan control, or granting of provisional consent:
- i) Subdivision or consent agreement;
 - ii) Condominium agreement;
 - iii) Site plan agreement;
 - iv) Purchase and sale agreements; and,
 - v) Covenants as per the Conservation Land Act registered under the Land Titles Act.
- o) *Development* that proposes the construction of a single-detached dwelling or an *accessory building or structure* on an *existing* lot of record will not be subject to the phosphorus load requirement.

13.1.7.4 Communication Technology

- a) Applications for *Major Development* shall be designed to accommodate infrastructure for the delivery of leading-edge communication technologies, including those required to deliver broadband services, in order to facilitate future advanced telecommunication capabilities and attract and maintain investment, facilitate research, development and knowledge-based initiatives, and support health services.
- b) Applications for *Major Development* will be required to provide a Communication Implementation Plan that demonstrates how communication infrastructure will be designed and implemented within the development, to the satisfaction of the Town.

13.1.8 IMPLEMENTATION

13.1.8.1 Authority

- a) The Town shall implement this Secondary Plan by utilizing the powers conferred upon it by the Planning Act, the Municipal Act and any other applicable statute or regulation.

13.1.8.2 Existing Non-Conforming Uses, Buildings and Structures

- a) *Existing* non-conforming uses, building and structures shall be regulated in accordance with the policies of Section 11.1 of the Georgina Official Plan.

13.1.8.3 By-Laws

- a) The Town's Comprehensive Zoning By-law along with site-specific Zoning By-law Amendments, including holding provisions, interim control by-laws and temporary use by-laws, shall be regulated in accordance with the policies of Section 11.2 of the Georgina Official Plan.
- b) Maintenance and Occupancy by-laws shall be regulated in accordance with the policies of Section 11.13 of the Georgina Official Plan.
- c) Municipal Act by-laws shall be regulated in accordance with the policies of Section 11.14 of the Georgina Official Plan.

13.1.8.4 Plans of Subdivision/Condominium and Consents

13.1.8.4.1 Plans of Subdivision/Condominium

- a) Plans of subdivision and condominium shall be regulated in accordance with Section 11.3 of the Georgina Official Plan. Notwithstanding Official Plan policy 11.3.4, the measures to be incorporated into subdivision and condominium agreements shall include:
 - i) Limiting the removal of trees, and restricting grading and soil compaction to the minimum necessary to carry out *development* activity;
 - ii) Restricting the removal of trees and vegetation prior to the approval of a Tree Preservation and Compensation Plan by the Town in consultation with the Lake Simcoe Region Conservation Authority, and the posting of securities by the proponent;
 - iii) The requirement for erosion and sediment control methods such as temporary erosion control mats and silt fences to control and convey stormwater run-off;
 - iv) Minimizing the deposit of deleterious substances in stormwater run-off;
 - v) Seeding or sodding exposed soils once construction is complete and seasonal conditions permit; and,

- vi) Ensuring erosion and sediment controls are implemented effectively and are adapted to the changing weather and site conditions.

13.1.8.4.2 Consents

- a) Generally, where no more than three lots are to be created, and where it is determined that a registered plan of subdivision is not required to ensure proper and orderly *development*, the land to be developed may be divided by consent. In determining whether a proposed land division should require a plan of subdivision or a consent to sever, the following criteria shall be applied:

- i) Whether the extension of an existing public road, opening of an unopened road allowance or the creation of a new road is required;
- ii) Whether the extension or expansion of municipal services is required; and,
- iii) Whether an agreement with appropriate conditions is required by the Town, Region or Province in respect of any part of the lands that would be defined as remaining lands in a consent application.

Where there is an affirmative answer to any of the above criteria, the proposed *development* should take place by plan of subdivision.

- b) Consents may be permitted in all land use designations for such reasons as the creation of a new lot, lot boundary adjustments, right-of-ways, easements, or to convey additional lands to an abutting lot, provided an undersized lot is not created.
- c) Land containing a detached *accessory building or structure* in which an *additional residential unit* is located will be prohibited from being severed from the property.
- d) Lot creation will not be permitted within *hazardous lands*.
- e) Applications for consents for all land use designations as shown on Schedule B will only be granted where:
 - i) It is clearly not in the public interest that a plan of subdivision be registered;
 - ii) The lot can be adequately serviced by roads, sanitary sewage disposal, water supply, and storm drainage facilities;
 - iii) No extension, improvement or assumption of municipal services is required;
 - iv) The lot will have adequate frontage on an open and assumed municipal road, and access will not result in traffic hazards;
 - v) The lot will not restrict the ultimate *development* of adjacent lands;
 - vi) The size and shape of the lot conforms to the Zoning By-law, is appropriate for the use proposed and is *compatible* with adjacent lots;
 - vii) The consent complies with all relevant permitted uses and policies of this Secondary Plan; and,
 - viii) The area's natural features, functions or ecological processes are not negatively affected.

13.1.8.5 Site Plan Control

- a) Site plan control is regulated in accordance with the policies of Section 11.5 of the Georgina Official Plan and the following policies.
- b) The Town may utilize the provisions of site plan control to the maximum extent permitted by the Planning Act, including, without limitation, the control of building materials, colour and architectural detail.
- c) In addition to those matters identified in the Official Plan, plans, drawings and elevations shall also address parking areas, walkways, fences and loading areas.

13.1.8.6 Community Improvement

- a) Community improvement shall be regulated in accordance with the policies of Section 8.6 of the Georgina Official Plan and the following policy.
- b) The Town may consider passing a *Community Improvement Project Area* by-law under Section 28(2) of the Planning Act, for all lands identified as within the Urban Centres designation.

13.1.8.7 Land Dedication, Acquisition and Securement

- a) Land dedication, acquisition and securement shall be regulated in accordance with the policies of Section 11.7 of the Georgina Official Plan and the following policies.
- b) Notwithstanding policy 11.7.1 of the Official Plan, where the opportunity arises, the Town may consider the acquisition of lands located along the Lake Simcoe shoreline, the Maskinonge River, and/or which form part of the Natural Heritage System and Parks Network. The Town shall encourage the voluntary dedication of such lands into public ownership for the purpose of providing public access to the water's edge, conservation or expanding the Natural Heritage System and Parks Network.
- c) Notwithstanding policy 11.7.2 of the Official Plan, the Town is committed to pursuing the dedication of conservation lands and easements containing *key natural heritage features* or *key hydrologic features* to protect and enhance the Natural Heritage System and Parks Network. Where appropriate, the Town also encourages the voluntary dedication of lands located within the Environmental Overlays associated with the *key natural heritage feature* or *key hydrologic feature*.

13.1.8.8 Asset Management Plan and Capital Works Program

- a) Asset management plans and capital works programs shall be regulated in accordance with the policies of Section 11.15 of the Georgina Official Plan and the following policy.
- b) In addition to the policies of the Official Plan, the construction of all public works shall be carried out in conformity with the policies of this Secondary Plan and have regard for the Urban Design and Architectural Control Guidelines attached to this Secondary Plan as Appendix I.

13.1.8.9 Site Alteration

- a) *Site alteration* shall be regulated in accordance with the policies of Section 4.3 of the Georgina Official Plan and the following policy.
- b) In addition to the policies of the Official Plan, *site alteration* shall generally not occur prior to *development* approval being granted, save and except for where a pre-servicing agreement is in place, where the *site alteration* is to remove contaminated soils from the property, or to alleviate a site-specific issue such as flooding or soil erosion.

13.1.8.10 Contaminated Lands

- a) Contaminated lands shall be regulated in accordance with the policies of Section 4.5 of the Georgina Official Plan.

13.1.8.11 Municipal Finance

13.1.8.11.1 Fiscal Responsibility

- a) The implementation of this Secondary Plan must be fiscally responsible by ensuring that the required capital expenditures to provide services to support *development* and infrastructure improvements are paid for in an equitable and appropriate manner. The Town will strive to maintain financial sustainability and integrity by managing its financial resources and by undertaking development in a fiscally responsible manner.
- b) The Town will undertake capital works projects and programs in accordance with the approved capital budget, to provide the municipal services and infrastructure necessary for new growth.
- c) Future *development* will be monitored to ensure that a balance is maintained between demands for service and the overall fiscal capacity of the Town.
- d) Where possible, the Town will use financial mechanisms available to it under any legislative authority, including the Municipal Act, Development Charges Act, Planning Act and any other applicable legislation, for the purposes of land use planning and the provision of municipal service infrastructure.
- e) The Town may request a Municipal Financial Impact Assessment from the proponent of any *development* application. The terms of reference for such study must be approved by the Town. The Study will be prepared, and may be peer reviewed, all at the proponent's expense. *Development* applicants may be refused or deferred on the basis of an unacceptable financial impact or burden on the Town, if suitable mitigation measures are not available.

13.1.8.11.2 Development Charges

- a) Development charges shall be regulated in accordance with the policies of Section 11.6.1 of the Georgina Official Plan and the Development Charges Act. In addition to the policies of the Official Plan, the Town may exempt or defer some or all of the development charge

or exempt certain *development* or *redevelopment* from the development charge as a means to promote specific *development*, *redevelopment* or revitalization objectives in accordance with this Secondary Plan.

13.1.8.11.3 Other Financial Requirements

- a) Other financial requirements shall be regulated in accordance with the policies of Section 11.6.2 of the Georgina Official Plan.

13.1.8.11.4 Developers Group Agreement

- a) Prior to the final approval of any large-scale *development* involving a number of separate properties and landowners, the Town may require that landowners with applications for *development* enter into an agreement or agreements to address the sharing of the common costs of *development*.

13.1.8.12 Development Review

- a) All *development* applications shall be subject to review in accordance with the policies of this Secondary Plan and the applicable policies of the Georgina Official Plan.

13.1.8.12.1 Amendments to this Secondary Plan

- a) Amendments to this Secondary Plan prior to the statutory review required by the Planning Act are discouraged. However, if amendments are considered, the following criteria and the pertinent policies of the Secondary Plan will provide a context for the review of the proposed amendment:
 - i) The impact of the proposed change on the ability of the Town to comply with the vision, guiding principles, objectives, intentions and policies contained in this Secondary Plan, or on other municipal policies, programs or interests;
 - ii) The need for the proposed change;
 - iii) The effect of the proposed change on Town and Regional services and facilities; and,
 - iv) The implications, if any, that the amendment may have for other policies and provisions of this Secondary Plan.
- b) An amendment to this Secondary Plan is required to permit the establishment of uses other than those permitted in this Secondary Plan. In considering an amendment to either the schedules or policies of this Secondary Plan, regard shall be had to the following criteria that are in addition to those specified elsewhere in this Secondary Plan:
 - i) The need for the proposed use;
 - ii) The extent to which the existing areas in the proposed categories are developed, and the nature and adequacy of such existing development;

- iii) The physical suitability of the land for such proposed use; and,
- iv) The location of the areas under consideration with respect to the potential impact on:
 - The adequacy of the existing and the potential impact upon the road network in relation to the *development* of such proposed areas;
 - The convenience and accessibility of the site for vehicular, bicycle and pedestrian traffic and the traffic safety in relation thereto;
 - The adequacy of the water supply and sewage disposal facilities, and other municipal services in view of the policies contained in this Secondary Plan and in accordance with technical reports or recommendations which shall be requested from the developer and subject to the approval of the Town, York Region, and any other applicable approval authority;
 - The compatibility of such proposed uses with uses in the surrounding area;
 - The financial position of the municipality;
 - Health outcomes; and,
 - The population level and the social character of the community.

13.1.8.12.2 Pre-Consultation and Submission Requirements

- a) Pre-consultation and submission requirements shall be regulated in accordance with the policies of Sections 10.1.1 and 10.1.2, respectively, of the Georgina Official Plan.

13.1.8.12.3 Complete Application Requirements

- a) Complete application requirements shall be regulated in accordance with the policies of Section 10.1.3 of the Georgina Official Plan.

13.1.8.12.4 Peer Review

- a) Peer review shall be regulated in accordance with the policies of Section 10.2 of the Georgina Official Plan.

13.1.8.12.5 Public Consultation

- a) Public consultation shall be regulated in accordance with the policies of Section 11.11 of the Georgina Official Plan.

13.1.8.12.6 Decision Making

- a) The Town will take a collaborative approach in the decision-making process through coordinating, where appropriate, with internal Town Departments, York Region, the Lake

Simcoe Region Conservation Authority, agencies, ministries, the Chippewas of Georgina Island First Nation, and the public.

13.1.8.13 Monitoring and Measuring Performance

- a) Monitoring and measuring performance shall be regulated in accordance with the policies of Section 11.17 of the Georgina Official Plan.

13.1.8.14 Phasing

- a) Growth and *development* shall occur in a phased manner that ensures the provision of a full range of services to residents. Factors for consideration are:
 - i) Positive integration with the existing urban structure of Keswick resulting in a contiguous and compact urban built form;
 - ii) The provision of adequate municipal services (e.g. water, sewer, stormwater, recreation) and transportation infrastructure to accommodate the proposed growth;
 - iii) The adequacy of *public service facilities* and other services (e.g. utilities, retail) to support residents; and,
 - iv) Alignment with the timing and phasing of Town and Region Master Plans and Capital Plans.

If one or more of these factors cannot be addressed satisfactorily, the processing of *development* applications may be deferred or held in abeyance until an appropriate service level can be provided.

13.1.9 INTERPRETATION

13.1.9.1 Transition

- a) Complete applications for *development* submitted to the Town prior to this Plan's adoption by Council shall not have to meet requirements of this Plan. Applicants will be encouraged to work with the Town to re-examine the proposed applications based on the policies of this Secondary Plan.
- b) Complete applications for *development* received on or after the date of this Secondary Plan's adoption by Council will be subject to the policies of this Secondary Plan.
- c) Legally *existing* and approved land uses shall continue to be recognized.
- d) *Existing development* approvals which are located in whole or in part within the Environmental Protection Area designation or within an Environmental Overlay, may be recognized without the requirement for an additional Environmental Impact Study. However, should a new, extended or amended planning approval or conditions of draft plan approval be required, at the discretion of the Town the proponent may be required to undertake a further Environmental Impact Study in accordance with the policies of this Secondary Plan.

13.1.9.2 Land Use Boundaries and Roads

- a) With the exception of the Secondary Plan Area boundary, it is intended that the boundaries of land use designations on Schedule A and Schedule B be considered as approximate and exact only where bounded by roads, rivers or streams or other similar physical or geographical demarcations. It is also intended that the location of roads and the cycling network, as indicated on Schedule E, be considered as approximate.
- b) Amendments to this Secondary Plan will not be required to permit minor adjustments to such approximate land use boundaries or to such approximate location of roads or the cycling network, provided that the general intent of this Secondary Plan is preserved. Such minor deviations will not be reflected in the Schedules.
- c) The consideration of an expansion to the boundaries of the Keswick Secondary Plan Area shall only be undertaken through a Municipal Comprehensive Review in accordance with provincial and regional planning requirements.

13.1.9.3 Stated Measurements and Quantities

- a) It is intended that all figures and quantities herein shall be considered as approximate only and not exact. Amendments to the Secondary Plan will not be required to permit a minor deviation from any of the stated quantities or figures.

13.1.9.4 Legislation

- a) Where this Secondary Plan makes reference to an Act of the Legislature or an Ontario Regulation, or to a provincial, regional or municipal plan, study, report or guideline, such reference shall include its successor upon amendment or replacement.

13.1.9.5 Definitions

Certain definitions used in this Secondary Plan have been taken from the Georgina Official Plan, the Sutton/Jackson's Point Secondary Plan (SJPS), and the following provincial and regional planning policy documents and are identified by their respective acronym in brackets at the end of the definition:

- Provincial Policy Statement, 2020 (PPS);
- A Place to Grow: Growth Plan for the Greater Golden Horseshoe, 2020 (GP);
- Greenbelt Plan, 2017 (GBP);
- Lake Simcoe Protection Plan, 2009 (LSPP); and,
- York Region Official Plan, 2010 (YROP).

Active Transportation

Means human-powered travel, including but not limited to, walking, cycling, inline skating and travel with the use of mobility aids, including motorized wheelchairs and other power-assisted devices moving at a comparable speed. (PPS)

Active Use

Means a use, generally commercial or retail in nature, which involves the frequent coming and going of the public and which adds to a dynamic streetscape through function and design.

Additional Residential Unit

Means a self-contained residential units with a private kitchen, bathroom facilities and sleeping areas within dwellings or within structures ancillary to a dwelling. *Additional residential units* are also referred to as second units, basement apartments, accessory apartments, granny flats, in-law apartments, or nanny suites.

Adjacent Lands

Means those lands contiguous to a *key natural heritage feature* or *key hydrologic feature* where it is likely that *development* or *site alteration* can reasonably be expected to have a negative impact on the feature. Generally, *adjacent lands* are considered to be within 120 metres from any part of the feature. (Based on PPS, from Georgina Official Plan)

Adverse Effects

As defined in the Environmental Protection Act, means one or more of:

- a) Impairment of the quality of the natural environment for any use that can be made of it;
- b) Injury or damage to property or plant or animal life;

- c) Harm or material discomfort to any person;
- d) An adverse effect on the health of any person;
- e) Impairment of the safety of any person;
- f) Rendering any property or plant or animal life unfit for human use;
- g) Loss of enjoyment of normal use of property; and,
- h) Interference with normal conduct of business. (PPS)

Accessory Building or Structure

Means a detached, subordinate building or structure on the same lot as the main building devoted exclusively to an *accessory use*. (Georgina Official Plan)

Accessory Uses

Means a use naturally and normally incidental, subordinate in purpose and floor area, and exclusively devoted to a main use of land, building or structure located on the same lot. (Georgina Official Plan)

Affordable

- a) In the case of ownership housing, the least expensive of:
 - i) Housing for which the purchase price results in annual accommodation costs which do not exceed 30 % of gross annual household income for low- and moderate- income households; or,
 - ii) Housing for which the purchase price is at least 10 % below the average purchase price of a resale unit in the regional market area;
- b) In the case of rental housing, the least expensive of:
 - i) A unit for which the rent does not exceed 30 % of gross annual household income for low- and moderate- income households; or,
 - ii) A unit for which the rent is at or below the average market rent of a unit in the regional market area.

For the purposes of this definition, low- and moderate- income households means, in the case of ownership housing, households with incomes in the lowest 60 % of the income distribution for the *regional market area*; or in the case of rental housing, households with incomes in the lowest 60 % of the income distribution for renter households for the regional market area. (PPS)

Angular Plane

Means a flat line extending from a lot line on the adjacent side of the road and projecting over a lot, at a specified angle measured up from the horizontal, which identifies the maximum permitted height of a building or structure on the lot.

Archaeological Resources

Means artifacts, archaeological sites, marine archaeological sites, as defined under the Ontario Heritage Act. The identification and evaluation of such resources are based upon archaeological fieldwork undertaken in accordance with the Ontario Heritage Act. (PPS)

Boarding House

Means an owner-occupied dwelling where guest rooms are offered, with or without meals, for long-term accommodation (generally more than one month). The dwelling may or may not be owner-occupied.

Built Heritage Resource

Means a building, structure, monument, installation or any manufactured remnant that contributes to a property's cultural heritage value or interest as identified by a community, including an Aboriginal community. *Built heritage resources* are generally located on property that has been designated under Parts IV or V of the Ontario Heritage Act, or included on local, provincial and/or, federal registers. (Growth Plan)

Co-Housing

Means an ownership model where residents purchase homes together, which can include arrangements where residents have their own suites and the rest of the house is shared common space, or where residents have their own self-contained units as well as access to shared resources and community space, which can include both indoor and outdoor amenities.

Community Housing

Means affordable housing owned and operated by non-profit housing corporations, housing co-operatives and municipal governments or district social services administration boards. These providers offer subsidized or low-end-of market rents.

Community Hub

Means a public space that brings several community agencies and neighbourhood groups together to offer a range of activities, programs and services.

Community Improvement Plan

Means a plan prepared and approved under Section 28 of the Planning Act. It is intended to promote activities that will improve the physical and functional quality of a defined *Community Improvement Project Area*. A *Community Improvement Plan* will specify the powers of Council with respect to carrying out the *Community Improvement Plan*, including powers to:

- a) Make grants and loans;
- b) Acquire, improve and develop land and buildings; and,
- c) Maintain, repair, rehabilitate, and/or construct infrastructure. (SJPS)

Community Improvement Project Area

Means an area that may be an entire municipality or an area within a municipality, the improvement of which, in the opinion of Council, is desirable because of age, dilapidation, overcrowding, faulty arrangement, unsuitability of buildings or for any other environmental, social or community economic development reason. (SJPS)

Compatible

In the context of *development*, *compatible* means *development* that may not necessarily be the same or similar to the existing buildings or uses in the area, but, nonetheless, enhances the established community and shall be able to coexist with buildings and uses in the vicinity without causing any adverse impacts on surrounding properties. (Georgina Official Plan)

Complete Community

Means places such as mixed-use neighbourhoods or other areas within cities, towns, and settlement areas that offer and support opportunities for people of all ages and abilities to conveniently access most of the necessities for daily living, including an appropriate mix of jobs, local stores, and services, a full range of housing, transportation options and *public service facilities*. A *complete community* is age-friendly and may take different shapes and forms appropriate to their contexts. (Growth Plan)

Complete Streets

Means streets planned to balance the needs of all road users, including pedestrians, cyclists, transit-users, and motorists. (Growth Plan)

Cool Roof

Means a roof that has been designed to reflect more sunlight and absorb less heat than a standard roof.

Cultural Heritage Landscape

Means a defined geographical area that may have been modified by human activity and is identified as having cultural heritage value or interest by a community, including an Indigenous community. The area may include features such as buildings, structures, spaces, views, archaeological sites or natural elements that are valued together for their interrelationship, meaning or association. *Cultural heritage landscapes* may be properties that have been determined to have cultural heritage value or interest under the Ontario Heritage Act or have been included on Federal and/or international registers, and/or protected through official plan, zoning by-law, or other land use planning mechanisms. (PPS)

Cultural Heritage Resources

Means *built heritage resources*, *cultural heritage landscapes* and *archaeological resources* that have been determined to have cultural heritage value or interest for the important contribution they make to our understanding of the history of a place, an event, or a people. While some *cultural heritage resources* may already be identified and inventoried by official sources, the significance of others can only be determined after evaluation. (Greenbelt Plan)

Day Care Facilities

Means buildings or structures or part thereof used for temporary educational, supervision, or personal care services for children, adults with special needs, or older persons during the day by a person or organization.

Delineated Built Boundary

Means the limits of the developed urban area as defined by the Province of Ontario in consultation with affected municipalities for the purpose of measuring the minimum *intensification* target in this Plan. (Growth Plan)

Delineated Built-up Area

Means all land within the *delineated built boundary*. (Growth Plan)

Designated Greenfield Area

Means lands within settlement areas (not including rural settlements) but outside of *Delineated Built-up Areas* that have been designated in an official plan for *development* and are required to

accommodate forecasted growth to the horizon of this Plan. *Designated Greenfield Areas* do not include excess lands. (Growth Plan)

Development

Means the creation of a new lot, a change in land use, or the construction of buildings and structures, requiring approval under the Planning Act, but does not include:

- a) Activities that create or maintain infrastructure authorized under an environmental assessment, Planning Act, or Condominium Act process; or,
- b) Works subject to the Drainage Act. (Growth Plan)

Edible Landscapes

Means where food plants are used as design features in a landscape. These plants are used both for aesthetic value as well as consumption.

Emergency Housing

Means a premises accommodating and providing temporary lodging, board, and personal support services to individuals experiencing homelessness in a 24-hour supervised setting.

Employment Land Jobs

Means jobs accommodated in primarily low-rise industrial- type building space, the vast majority of which are located within business parks and industrial areas. This includes largely industrial sector employment, including manufacturing, processing, research and development, wholesale trade, transportation and warehousing, communications facilities, offices and associated retail and ancillary facilities.

Environmentally Significant Areas (ESAs)

Means land or water areas that contain distinctive or unusual features, perform a key ecological function and/or provide habitat for significant plant and/or animal species as determined by studies undertaken from time to time by the Lake Simcoe Region Conservation Authority and/or the Town. (YROP)

Existing

Means legally existing as of the date of the adoption of this Plan by Council and which has continued to exist to present. (Georgina Official Plan)

Fish Habitat

Means fish habitat as defined in the Federal Fisheries Act as spawning grounds and nursery, rearing food supply, and migration areas on which fish depend directly or indirectly in order to carry out their life process. (YROP)

Garden Suite

A one-unit detached residential structure containing bathroom and kitchen facilities that is ancillary to an existing residential structure on the same lot and that is designed to be portable.

Green Infrastructure

Means natural and human-made elements that provide ecological and hydrologic functions and processes. *Green infrastructure* can include components such as natural heritage features and systems, parklands, stormwater management systems, street trees, urban forests, natural channels, permeable surfaces, and green roofs. (PPS)

Gross Floor Area

Means the sum of the total area of each floor level of a building, above and below the ground, measured from the exterior of the main wall of each floor level.

Ground-Oriented

With respect to residential buildings, means housing built forms and typologies where each principal unit has its own entrance on the ground floor of a building.

Group Home

Means a residence licensed or funded under a federal or provincial statute for the accommodation of three to eight persons, exclusive of staff, living under supervision in a single housekeeping unit and who, by reason of their emotional, mental, social or physical condition, require a group living arrangement for their well being, but does not include a *halfway house*.

Habitat of Endangered Species, Threatened Species, and Special Concern Species

Means habitat for endangered, threatened or special concern species are identified by regulation under the Endangered Species Act. (YROP)

Halfway House

Means a facility funded, licensed, approved, or supervised by the Province of Ontario as a detention or correctional facility under any general or special Act and Amendments or replacements thereto, for the accommodation of three or more residents, exclusive of staff.

Hazardous Lands

Means property or lands that could be unsafe for *development* due to naturally occurring processes. Along the shorelines of large inland lakes, this means the land, including that covered by water, between a defined offshore distance or depth and the furthest landward limit of the flooding hazard, erosion hazard or dynamic beach hazard limits.

Along river, stream and small inland lake systems, this means the land, including that covered by water, to the furthest landward limit of the flooding hazard or erosion hazard limits.

Home Occupation

Means an occupation conducted for gain or profit as an *accessory use* within a dwelling or an *accessory building or structure* on the same property. *Home occupations* are small-scale homebased businesses allowing people to work from their homes while not detracting from the residential character of the neighbourhood in which they live. Such uses may include hairdressing, dog grooming or music instruction. The retail sale of goods or wares shall not be permitted as a primary *home occupation* use. (modified from Georgina Official Plan)

Homes for Special Care

Means housing for adults over 18 with a psychiatric diagnosis who require supervision and ongoing support with activities of daily living. *Homes for special care* are residential settings which are licensed by the Ministry of Health and Long Term Care.

Home-Zones

Means streets in which the travelled portion of the road is shared between motor vehicles and pedestrians, with priority given to pedestrians. An example of a home-zone would include a portion of residential area where the maximum speed limit is under 15 km/hr and vehicles must

yield to pedestrians and cyclists. The purpose of *home-zones* is to change the way that streets are used by making them places for people, not just for traffic.

Housing Options

Means a range of housing types such as, but not limited to single-detached, semi-detached, rowhouses, *townhouses*, multiplexes, *additional residential units*, tiny homes, multi-residential buildings. The term can also refer to a variety of housing arrangements and forms such as, but not limited to life lease housing, *co-housing*, co-operative housing, community land trusts, land lease community homes, *affordable housing*, *special needs housing*, and housing related to employment, institutional or educational uses.

Infill

Means any *development* that occurs within an established area, either on a vacant lot, or through redeveloping an existing building.

Intensification

Means the *development* of a property, site or area at a higher density than currently exists through:

- a) *Redevelopment*, including the reuse of brownfield sites;
- b) The *development* of vacant and/or underutilized lots within previously developed areas;
- c) *Infill development*; and,
- d) The expansion or conversion of existing buildings. (PPS)

Intermittent Stream

Means watercourses that are dry at times of the year, generally flowing during wet seasons but not the entire year. (YROP)

Kettle Lakes

Means a depression formed by glacial action and permanently filled with water. (YROP)

Key Hydrologic Features

Means the following features:

- a) *Wetlands*;
- b) *Lakes and their littoral zones*;
- c) *Kettle lakes*;
- d) *Permanent steam and intermittent stream*;
- e) *Seepage areas and springs*; and,
- f) *Lake Simcoe shoreline*. (YROP)

Key Natural Heritage Features

Means the following features:

- a) *Habitat of endangered species, threatened species and special concern species*;
- b) *Fish habitat*;
- c) *Wetlands*;

- d) *Life Science Areas of Natural and Scientific Interest (ANSIs),*
- e) *Environmentally Significant Areas (ESAs)*
- f) *Significant valleylands;*
- g) *Significant woodlands;*
- h) *Significant wildlife habitat; and,*
- i) *Sand barrens, savannahs, and tallgrass prairies. (YROP)*

Lakes and their Littoral Zones

Means any inland body of standing water larger than a pool or pond or a body of water filling a depression in the earth's surface. The littoral zone refers to the area of shallow water in a lake that extends from the shoreline lakeward to the limit occupancy of rooted aquatic plants. (YROP)

Lake Simcoe Shoreline

Means the mark made by the action of water under natural conditions on the shore or bank of Lake Simcoe which action is so common and usual and so long continued that it has created a difference between the character of the vegetation or soil on one side of the mark and the character of the vegetation or soil on the other side of the mark. (LSPP)

Life Science Areas of Natural and Scientific Interest

Means areas of land and water containing natural landscapes or features that have been identified by the Province using approved evaluation procedures, as having life science values related to protection, scientific study or education. (YROP)

Live-Work Unit

Means a unit which contains separate residential and non- residential areas intended for both residential and non-residential uses concurrently, and shares a common wall or floor with direct access between the residential and non-residential areas.

Local Strategic Growth Areas

Means nodes, corridors, and other areas that have been identified by the Town to be the focus for accommodating *intensification* and higher-density mixed uses in a more compact built form. *Local Strategic Growth Areas* are identified on Schedule A.

Lodging Houses

Means any house or other building or portion of it in which four (4) or more persons are lodged for hire, but does not include a hotel, hospital, long term care home, home for the young or institution if it is licensed, approved or supervised under an Act, other than the Municipal Act.

Low Impact Development

Means a stormwater management strategy that is intended to mitigate the impacts of increased run-off and pollution by managing run-off as close to its source as possible. *Low Impact Development* comprises a set of site design strategies that promote infiltration, filtration, evapotranspiration, rainwater harvestation, and stormwater detention. Design strategies include, but are not limited to, bio-swales, rain barrels, bio-retention facilities, vegetated rooftops, permeable pavements and perforated pipe systems. (Georgina Official Plan)

Major Development

Means *development* consisting of:

- a) The creation of four or more lots;
- b) The construction of a building or buildings with a ground floor area of 500 m² or more; or,
- c) The establishment of a major recreational use. (LSPP)

Major Facilities

Means facilities which may require separation from *sensitive land uses*, including but not limited to airports, manufacturing uses, transportation infrastructure and corridor, rail facilities, marine facilities, sewage treatment facilities, waste management systems, oil and gas pipelines, industries, energy generation facilities and transmission systems, and resource extraction activities. (PPS)

Multimodal

Means relating to the availability or use of more than one form of transportation, such as vehicles, walking, cycling, buses, rapid transit, rail (such as commuter and freight), trucks, air, and marine. (Growth Plan)

Net Residential Hectare

Means residential density calculated on the basis of the *net residential land* area. (SJPSPP)

Net Residential Land

Means the area of residential use, excluding road allowances and the land area of all other land uses necessary to the basic residential use such as parks, open space, greenlands, stormwater management areas, churches, local commercial, schools, etc. (SJPSPP)

Parking Facilities

Means a structure or an area providing for the parking of vehicles. This definition does not include areas for the parking of vehicles associated with individual low-rise residential uses.

Places of Worship

Means a building, structure or part thereof, which is primarily used for the practice of religion and faith-based spiritual purposes wherein people assemble for religious worship, faith-based teaching, fellowship and community social outreach.

Population-Related Employment

Means employment that exists in response to a resident population that is primarily not located in employment areas or major office parks. This category includes retail and service commercial, education, health care, local government and work at home employment.

Priority Stormwater Management Works

Means stormwater management works that satisfy criteria established by the Ministry of Environment as may be amended from time to time. Such criteria may identify works based on a range of matters including, the size and type of development serviced by the works, the location of the works, any relevant subwatershed evaluations or stormwater management master plans, if available and the results of the phosphorous reduction strategy. (LSPP)

Public Service Facilities

Means land, buildings and structures for the provision of programs and services provided or subsidized by a government or other body, such as social assistance, recreation, police and fire protection, health and educational programs, long-term care services and cultural services. *Public service facilities* do not include infrastructure. (PPS)

Redevelopment

Means the creation of new units, uses or lots on previously developed land in existing communities, including brownfield sites. (PPS)

Regional Market Area

Means an area that has a high degree of social and economic interaction. The upper- or single-tier municipality, or planning area, will normally serve as the *regional market area*. However, where a *regional market area* extends significantly beyond upper- or single-tier boundaries, then the regional market area may be based on the larger market area. Where regional market areas are very large and sparsely populated, a smaller area, if defined in an official plan, may be utilized. (PPS)

Rooming House

Means a dwelling where guest rooms are offered, with or without meals, for long-term accommodation (generally more than one month). The dwelling may, or may not be owner-occupied.

Sand Barrens

Means land (not including land that is being used for agricultural purposes or no longer exhibits sand barrens characteristics) that:

- a) Has sparse or patchy vegetation that is dominated by plants that are:
 - (i) Adapted to severe drought and low nutrient levels; and,
 - (ii) Maintained by severe environmental limitations such as drought, low nutrient levels and periodic disturbances such as fire; and,
- b) Has less than 25 percent tree cover; and,
- c) Has sandy soils (other than shorelines) exposed by natural erosion, depositional process or both; or,
- d) Has been further identified, by the Ministry of Natural Resources or by any other person, according to evaluation procedures established by the Ministry of Natural Resources, as amended from time to time. (YROP)

Savannah

Means land (not including land that is being used for agricultural purposes or no longer exhibits savannah characteristics) that:

- a) Has vegetation with a significant component of non-woody plants, including tallgrass prairie species that are maintained by seasonal drought, periodic disturbances such as fire, or both; and,
- b) Has from 25 percent to 60 percent tree cover; and,
- c) Has mineral soils; or,

- d) Has been further identified, by the Ministry of Natural Resources or by any other person, according to evaluation procedures established by the Ministry of Natural Resources, as amended from time to time. (YROP)

Seepage Areas and Springs

Means sites of emergence of groundwater where the water table is present at the ground surface. Seepage areas are areas where groundwater emerges from the ground over a diffuse area. Springs are points of natural, concentrated discharge of groundwater. (YROP)

Senior Care Facility

Means supervised living accommodation that may include associated support services, and:

- a) Is licenced or funded under Province of Ontario or Government of Canada legislation;
- b) Is for persons requiring semi-independent or supervised group living arrangements; and,
- c) Is for more than 10 persons, exclusive of staff.

Sensitive Land Use

Means buildings, amenity areas, or outdoor spaces where routine or normal activities occurring at reasonably expected times would experience one or more *adverse effects* from contaminant discharges generated by a nearby *major facility*. *Sensitive land uses* may be a part of the natural or built environment. Examples may include, but are not limited to: residences, day care centres, and educational and health facilities. (PPS)

Short-term Rental Accommodation

Means a Short-term rental accommodation as defined by the Town of Georgina Short-term Rental Accommodation Licensing By-law, as amended, revised or replaced.

Significant Wildlife Habitat

Means areas where plants, animals and other organisms live, and find adequate amounts of food, water, shelter and space needed to sustain their populations. Specific wildlife habitats of concern may include areas where species concentrate at a vulnerable point in their annual or life cycle; and areas which are important to migratory or non-migratory species. These areas are ecologically important in terms of features, functions, representation or amount, and contribute to the quality and diversity of an identifiable geographic area or natural heritage system. The Provincial Significant Wildlife Habitat Technical Guide should be referenced to determine *significant wildlife habitat*. (YROP)

Significant Valleylands

Means a natural area that occurs in a valley or other landform depression that has water flowing through or standing for some period of the year. These areas are ecologically important in terms of features, functions, representation or amount. (YROP)

Significant Woodlands

Means woodlands which meet any one of the following criteria:

- a) Contains globally or provincially rare plants, animals or communities as designated by the Natural Heritage Information Centre;
- b) Contains species designated by the Committee of the Status of Endangered Wildlife in Canada or by the Committee on the Status of Species at Risk in Ontario as threatened, endangered, or of special concern;

- c) Is within 30 metres of a *key hydrologic feature*;
- d) Is over 2 hectares and:
 - (i) Is within 100 metres of another *key natural heritage feature* or *key hydrologic feature*; or,
 - (ii) Occurs within the Regional Greenlands System; and,
- e) Is north of the Oak Ridges Moraine and is 10 hectares or larger. (YROP)

Site Alteration

Means activities such as grading, excavation and the placement of fill that would change the landform and natural vegetative characteristics of a site. (PPS)

Special Needs Housing

Means any housing, including dedicated facilities, in whole or in part, that is used by people who have specific needs beyond economic needs, including but not limited to, needs such as mobility requirements or support functions required for daily living. Examples of special needs housing may include, but are not limited to long-term care homes, adaptable and accessible housing, and housing for persons with disabilities such as physical, sensory or mental health disabilities, and housing for older persons. (Georgina Official Plan)

Tallgrass Prairies

Means land (not including land that is being used for agricultural purposes or no longer exhibits tallgrass prairie characteristics) that:

- a) Has vegetation dominated by non-woody plants, including tallgrass prairie species that are maintained by seasonal drought, periodic disturbances such as fire, or both;
- b) Has less than 25 per cent tree cover;
- c) Has mineral soils; and,
- d) Has been further identified, by the Ministry of Natural Resources or by any other person, according to evaluation procedures established by the Ministry of Natural Resources, as amended from time to time.

Townhouse

Means any townhouse type or built form, provided that it is in accordance with the policies of this Secondary Plan, including with respect to height and density.

Transit-Supportive

Means *development* that makes transit viable and improves the quality of the experience of using transit. It often refers to compact, mixed-use development that has a high level of employment and residential densities. *Transit-supportive* development will be consistent with Ontario's Transit Supportive Guidelines. (Growth Plan)

Transportation Demand Management

Means a set of strategies that result in more efficient use of the transportation system by influencing travel behaviour by mode, time of day, frequency, trip length, regulation, route, or cost. (PPS)

Vegetation Protection Zone

Means a vegetated buffer area surrounding *key natural heritage feature* or a *key hydrologic feature*. These areas protect the feature and its functions from the impacts of land use changes and associated activities that will occur before, during and after construction, and where possible, restore or enhance the features and its functions. (YROP)

Wetlands

Means lands that are seasonally or permanently covered by shallow water, as well as lands where the water table is close to or at the surface. In either case the presence of abundant water has caused the formation of hydric soils and has favoured the dominance of either hydrophytic plants or water tolerant plants. The four major types of *wetlands* are swamps, marshes, bogs and fens.

Wetlands on the Greenbelt and within the Lake Simcoe Watershed, include any *wetlands* identified by the Ministry of Natural Resources or by any other person, according to evaluation procedures established by the Ministry of Natural Resources, as amended from time to time. (YROP)

Woonerf

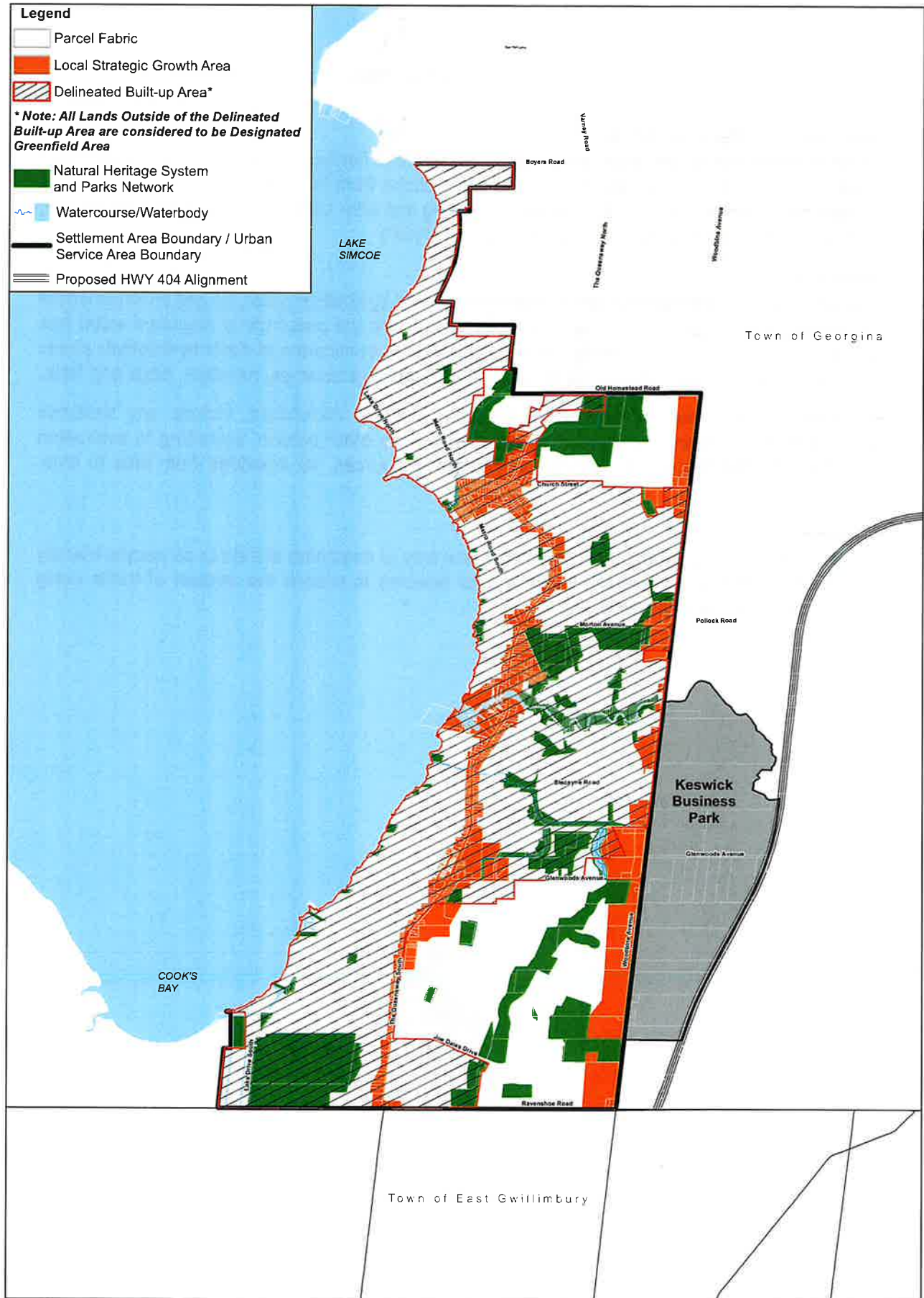
Means “living street” in Dutch, and refers to a new way of designing streets to be people-friendly open spaces. Roads are designed with special features to reduce the amount of traffic using them, or to make the traffic go slower.

Legend

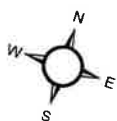
- Parcel Fabric
- Local Strategic Growth Area
- Delineated Built-up Area*

*** Note: All Lands Outside of the Delineated Built-up Area are considered to be Designated Greenfield Area**

- Natural Heritage System and Parks Network
- Watercourse/Waterbody
- Settlement Area Boundary / Urban Service Area Boundary
- Proposed HWY 404 Alignment



Data Source:
This map was derived from data provided by the Town of Georgina, the Regional Municipality of York and the LSRCA.



August 17, 2022

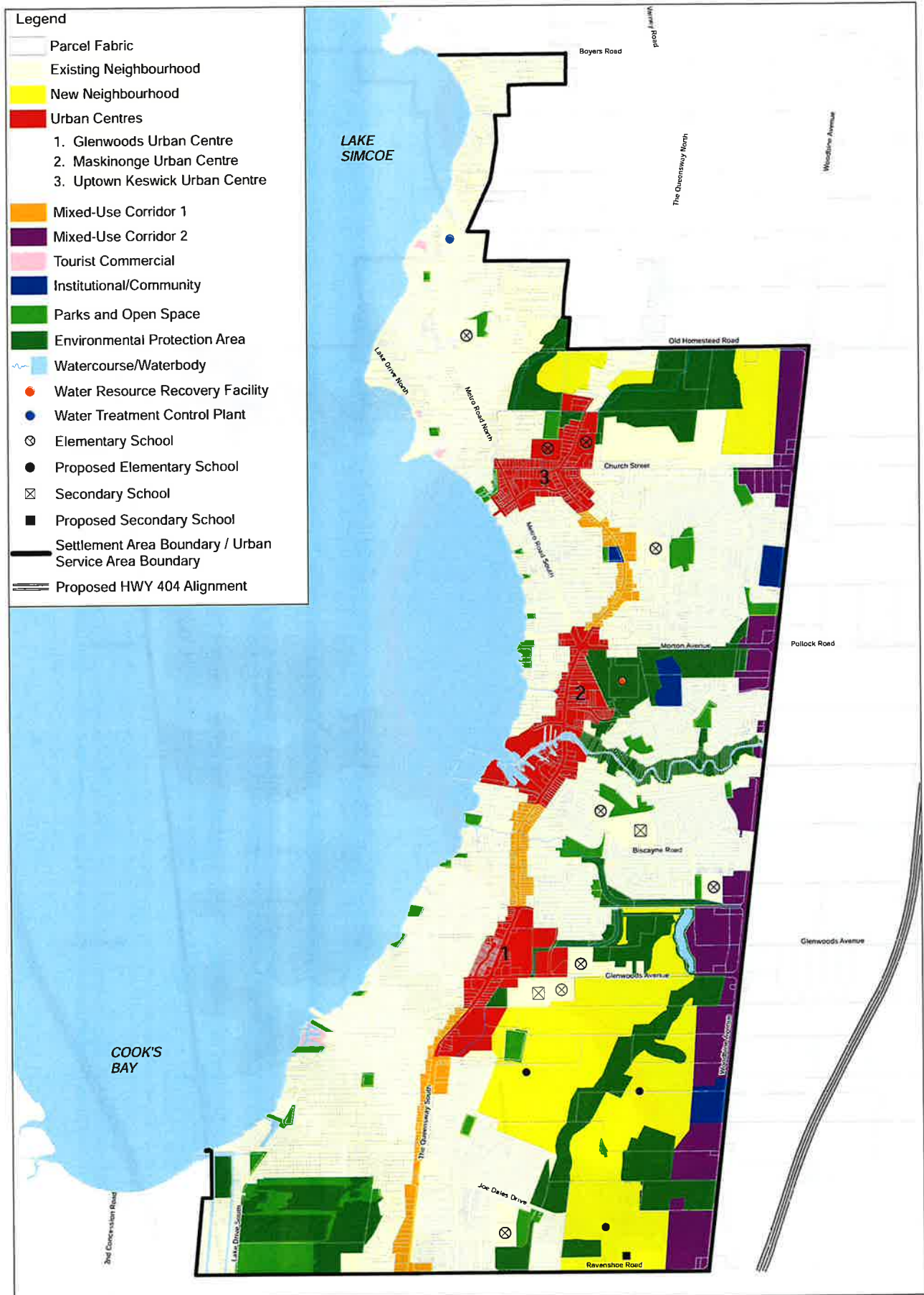
Schedule A: Growth Management

KESWICK SECONDARY PLAN

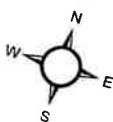
Town of Georgina

0 250 500 1,000 1,500 2,000 2,500 Metres





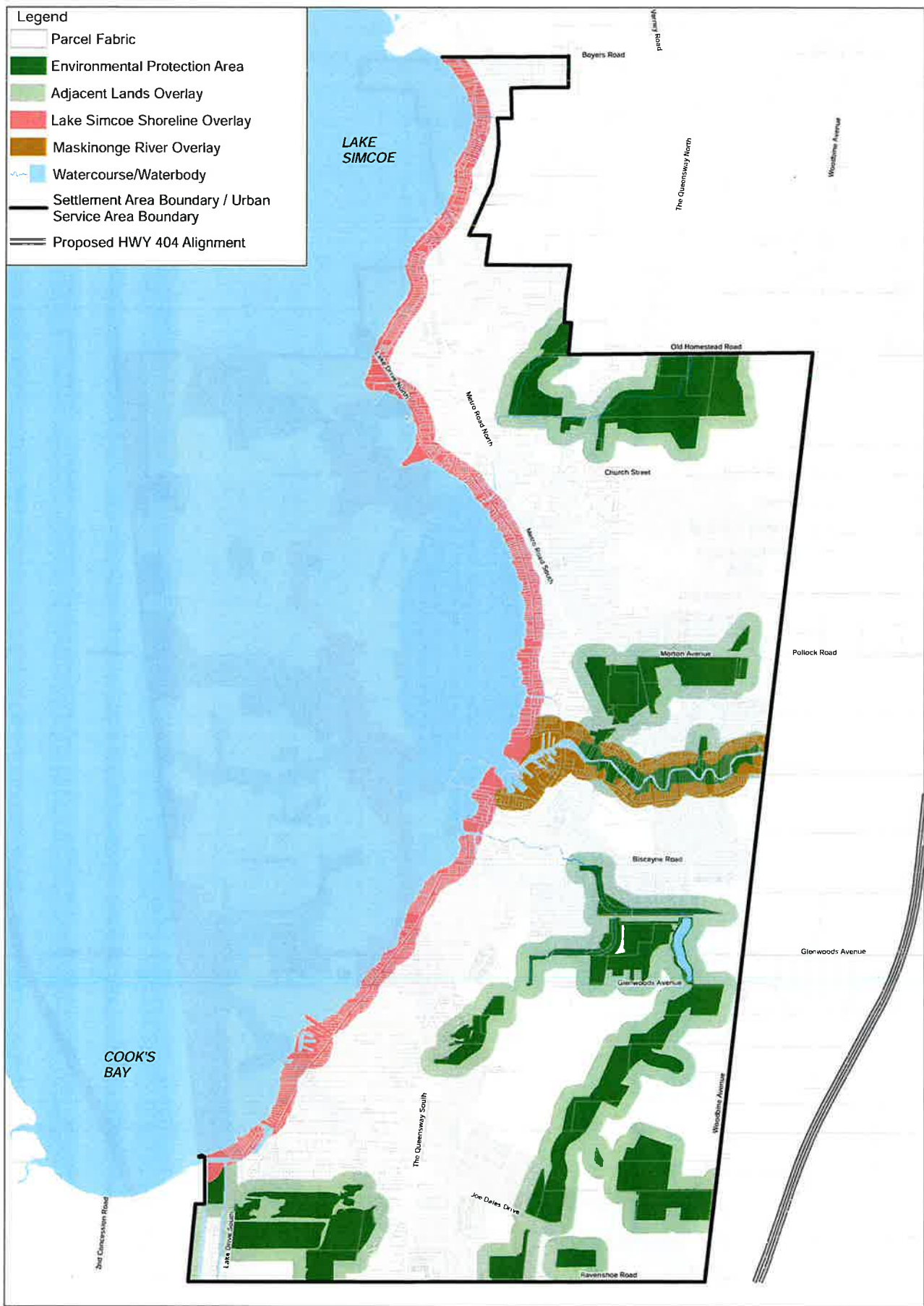
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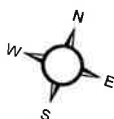
August 11, 2022

Schedule B: Land Use Plan KESWICK SECONDARY PLAN Town of Georgina

0 250 500 1,000 1,500 2,000 Metres



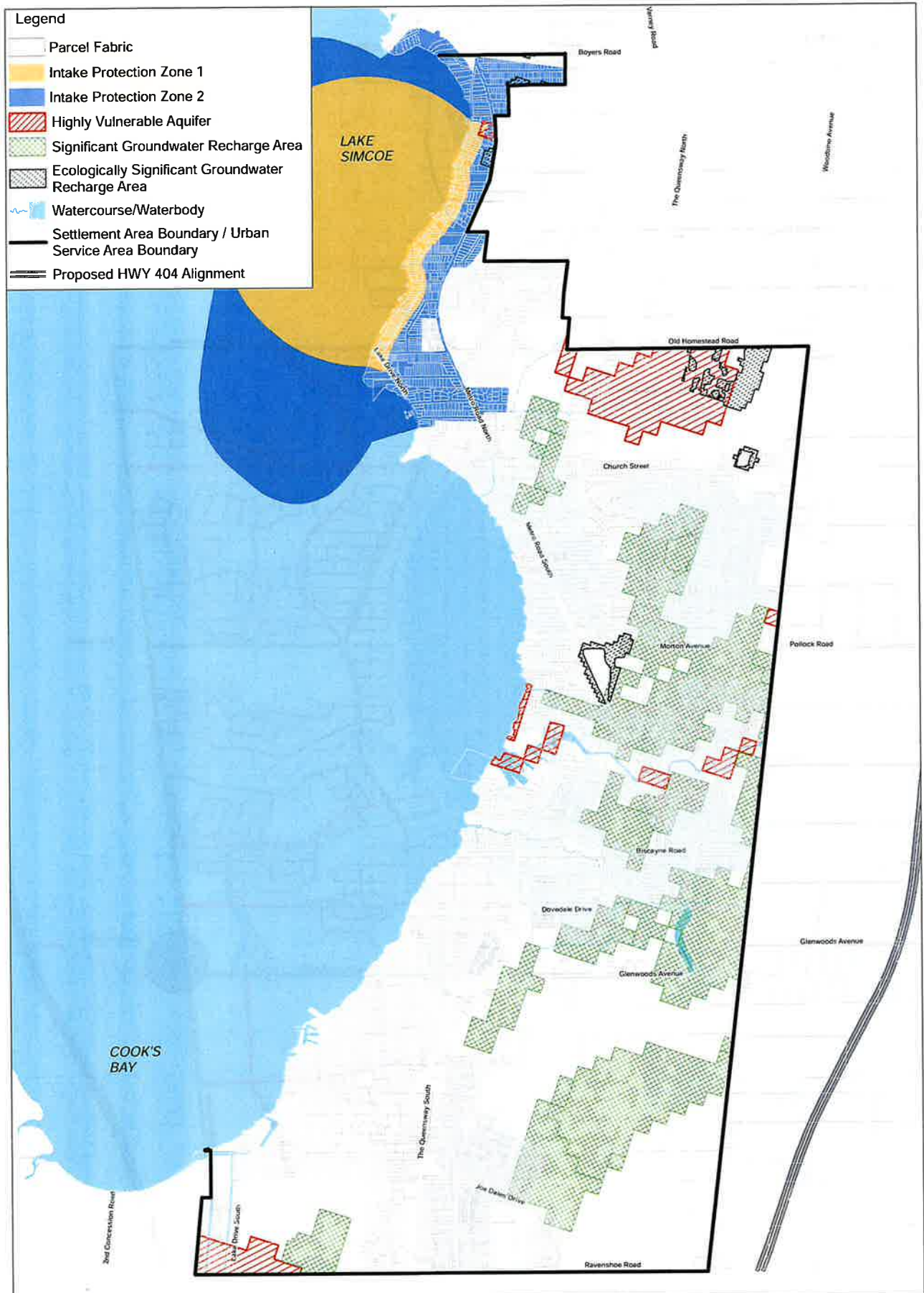
Map Source:
This map was derived from data provided by the Town of Georgina, the Regional Municipality of York and the LGCA.



August 17, 2022

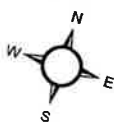
Schedule C: Environmental Overlays KESWICK SECONDARY PLAN Town of Georgina

0 250 500 1,000 1,500 2,000 Metres



Data Source:
This map was prepared from data provided to
the Town of Georgina and Regional Municipality
of York and the LSCA.

Schedule D: Source Water Protection Areas KESWICK SECONDARY PLAN Town of Georgina



August 11, 2022

0 250 500 1,000 1,500 2,000 Metres

Plan-It Geographical
PLAN Natural Heritage

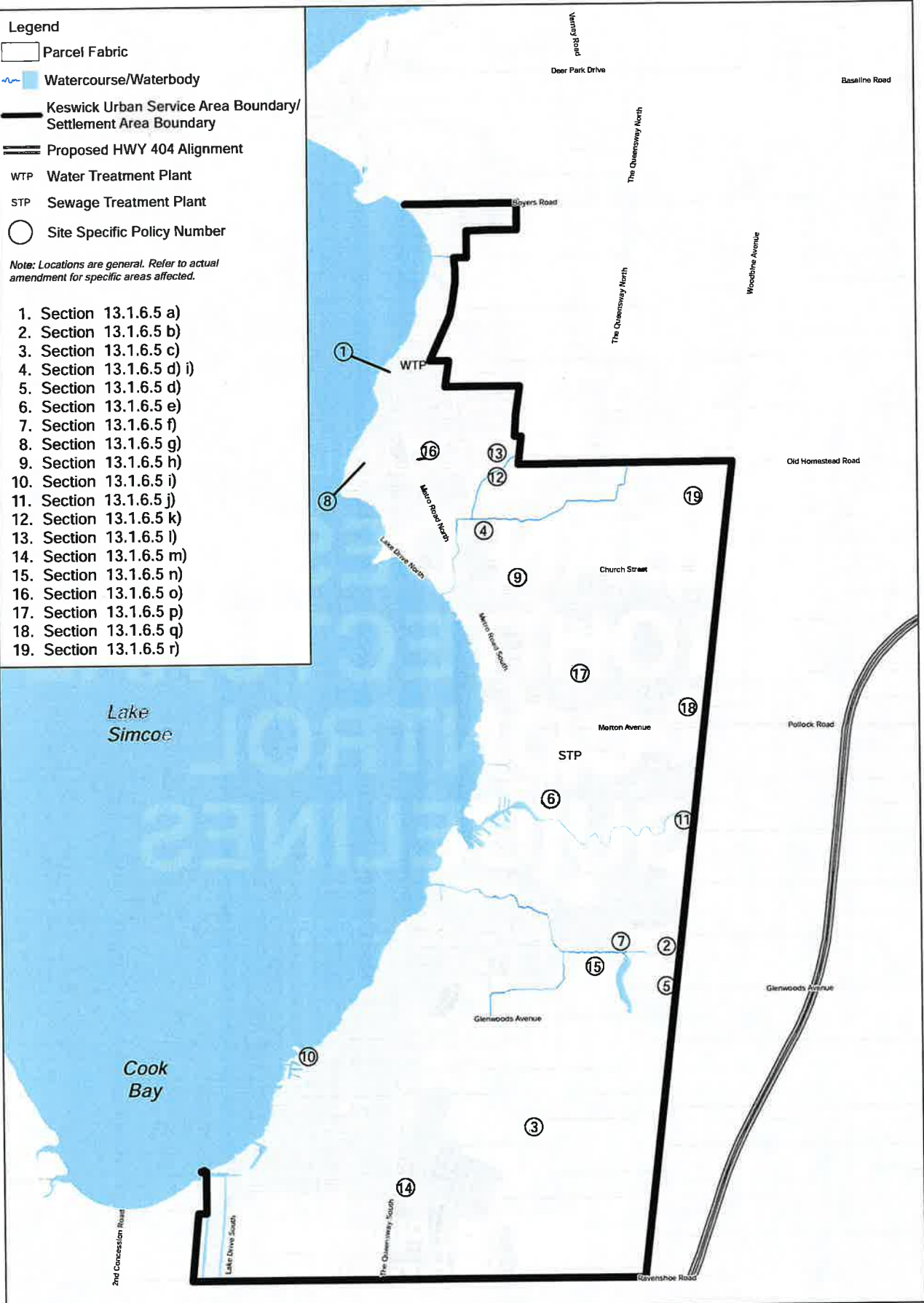
The Planning
Partnership

Legend

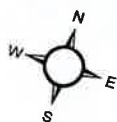
- Parcel Fabric
- Watercourse/Waterbody
- Keswick Urban Service Area Boundary/
Settlement Area Boundary
- Proposed HWY 404 Alignment
- WTP Water Treatment Plant
- STP Sewage Treatment Plant
- Site Specific Policy Number

Note: Locations are general. Refer to actual amendment for specific areas affected.

1. Section 13.1.6.5 a)
2. Section 13.1.6.5 b)
3. Section 13.1.6.5 c)
4. Section 13.1.6.5 d) i)
5. Section 13.1.6.5 d)
6. Section 13.1.6.5 e)
7. Section 13.1.6.5 f)
8. Section 13.1.6.5 g)
9. Section 13.1.6.5 h)
10. Section 13.1.6.5 i)
11. Section 13.1.6.5 j)
12. Section 13.1.6.5 k)
13. Section 13.1.6.5 l)
14. Section 13.1.6.5 m)
15. Section 13.1.6.5 n)
16. Section 13.1.6.5 o)
17. Section 13.1.6.5 p)
18. Section 13.1.6.5 q)
19. Section 13.1.6.5 r)



Data Source:
This map was derived from data provided by
the Town of Georgina, the Regional Municipality
of York and the LSRA.



August 17, 2022

0 300 600 1,200 1,800 2,400 Metres

Schedule F: Keswick Site Specific Exceptions

KESWICK SECONDARY PLAN

Town of Georgina

APPENDIX I

KESWICK SECONDARY PLAN

URBAN DESIGN & ARCHITECTURAL CONTROL GUIDELINES

August • 2022

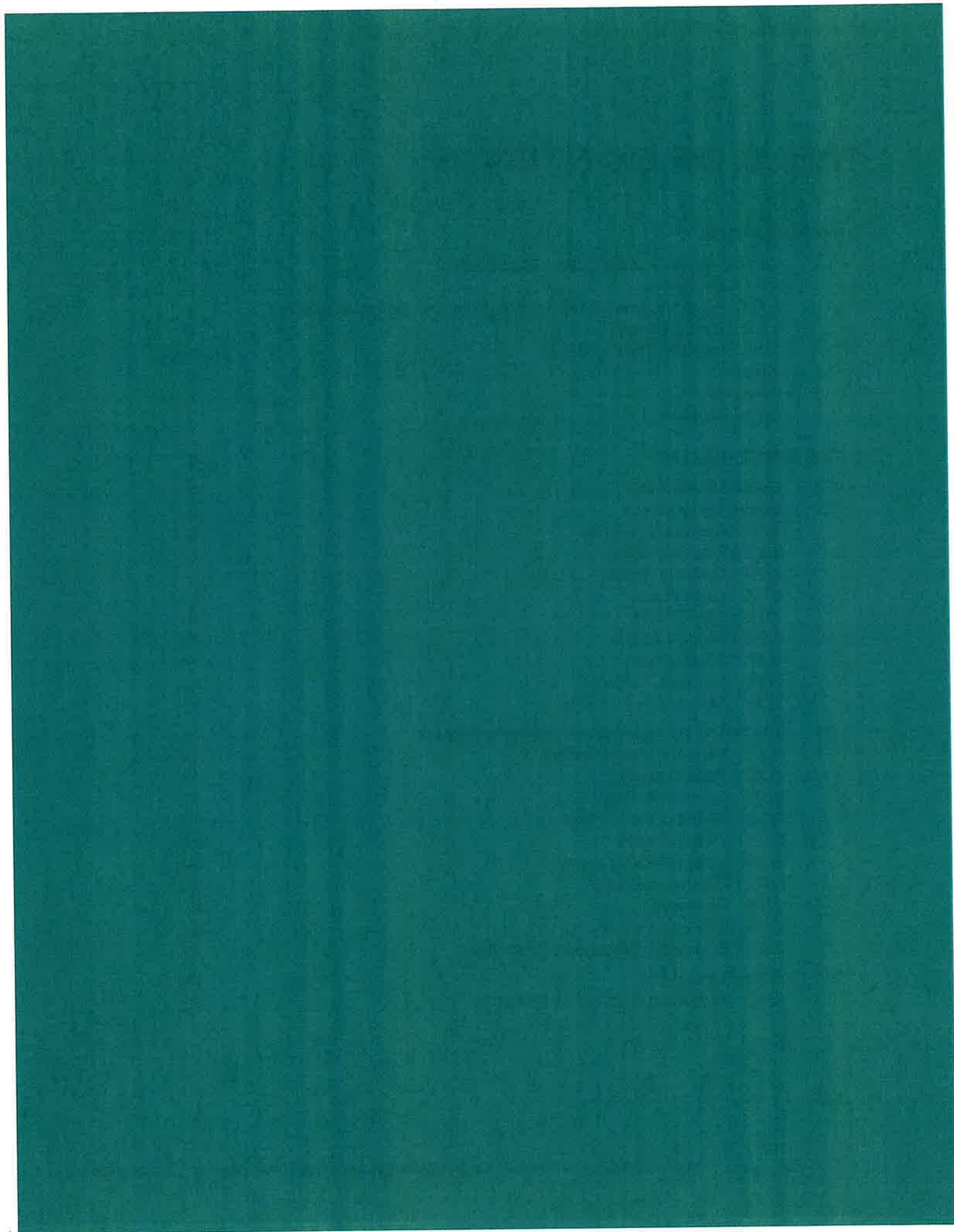


TABLE OF CONTENTS

1. INTRODUCTION	1
1.1 Context and Role of the Design Guidelines	1
1.1.1 Context	1
1.1.2 What Are Urban Design and Architectural Control Guidelines?	1
1.1.3 Zoning By-law	1
1.1.4 How Will They be Used?	1
1.2 Vision	2
1.3 Principles	2
1.4 Urban Structure	3
2. THE PUBLIC REALM	5
2.1 General Guidelines	5
2.2 Guidelines for Roads	6
2.2.1 General Guidelines	6
2.2.2 Regional Roads	8
2.2.3 Collector Roads	8
2.2.4 Local Roads	9
2.2.5 Main Streets	9
2.2.6 Window Roads	10
2.2.7 Private Roads	10
2.2.8 Municipal Lanes	11
2.2.9 Streetscape Elements	12
2.3 The Natural Heritage System and Parks Network	14
2.3.1 Natural Heritage System	14
2.3.2 Parks Network	16
2.3.3 Community Parks	17
2.3.4 Neighbourhood Parks	18
2.3.5 Village Greens	19
2.3.6 Open Space Linkages	20
2.3.7 Urban Agriculture	20
2.3.8 Gateways	21
2.3.9 Views and Vistas	21
2.3.10 Stormwater Management Facilities	22
2.4 Active Transportation	24
2.4.1 Pedestrian and Cycling System	24

3. THE PRIVATE REALM	27
3.1 General Guidelines for all Development	28
3.1.1 Urban Centres and Mixed Use Corridors	28
3.1.2 Neighbourhoods	29
3.1.3 Cultural Heritage	30
3.2 Site Planning	30
3.2.1 Site Layout	30
3.2.2 General Site Landscaping	31
3.2.3 Fences and Walls	33
3.3 Low-Rise Buildings	34
3.3.1 General Guidelines	34
3.3.2 Typologies	34
3.3.3 Siting and Setbacks	38
3.3.4 Private Outdoor Amenity Space	38
3.3.5 Garages and Driveways	39
3.3.6 Priority Lots	42
3.3.7 Building Design	45
3.4 Mid-Rise and High-Rise Buildings	49
3.4.1 Typologies	49
3.4.2 Siting and Setbacks	51
3.4.3 Access, Parking and Servicing	53
3.4.4 Building Design	54
3.4.5 Private Outdoor Amenity Space	55
3.5 Commercial and Mixed Use Buildings	58
3.5.1 Building Design	58
3.5.2 Parking	62
3.5.3 Servicing, Storage and Loading	64
3.6 Public Service Facilities	64
3.6.1 General Guidelines	64
3.6.2 Building Design	65
3.6.3 Schools Sites	66
3.6.4 Fire Stations	67
3.6.5 Places of Worship	67

4. GREEN INFRASTRUCTURE AND BUILDING	69
4.1 Energy Conservation	69
4.2 Water Use and Management	71
4.3 Lighting	73
4.4 Green Buildings/Green Site	73
4.5 Stewardship and Education	74
4.6 Retrofitting Existing Private Properties	75



1. INTRODUCTION

1.1 Context and Role of the Guidelines

1.1.1 Context

The Keswick Secondary Plan is the guiding document used to direct and manage growth in Keswick. It articulates the vision and guiding principles for how the community should be developed and outlines the policies for how land in the community should be used.

The Secondary Plan helps to ensure that future planning and development meets the specific needs of the community. The Secondary Plan addresses topics such as:

- Where new housing, offices and shops will be located;
- What services like roads, watermains, sewers, parks, and schools will be needed;
- When, how, and in what order, parts of the community will grow; and,
- Community improvement initiatives.

The Town recognizes that one of the key objectives for the successful evolution and development of the community is ensuring design excellence.

1.1.2 What Are Urban Design and Architectural Control Guidelines?

Urban Design and Architectural Control Guidelines ("the Guidelines") are statements that include design guidance, criteria, and standards for how to shape the built environment, both the individual elements, as well as how these should be spatially arranged and relate to one another. The Guidelines address diverse scales of development, from site specific to community-wide and typically address the design of buildings and landscape features, their organization within a defined area, and their relationship to their built and natural surroundings.

1.1.3 Zoning By-law

The Zoning by-law addresses matters such as lot coverage, parking, setbacks and height - the quantitative aspects of a community's physical form. While zoning regulates how buildings sit within a lot or block, it represents only one of the planning tools that may be used to guide and shape development. Zoning is best used in conjunction with the Guidelines to create development that promotes design excellence, and is compatible with, and fits within its surrounding context.

The Guidelines address the relative height, massing, and articulation of buildings and landscapes, and their relationship to one another and to their surroundings. These qualitative aspects of physical form work in combination with zoning parameters to lend shape and character to a community.

1.1.4 How Will They be Used?

The Urban Design and Architectural Control Guidelines:

- Will be used as the basis for the creation of development specific Urban Design and Architectural Control Guidelines, which in turn will be implemented through the site plan approval process or a control architect at the building permit stage of a subdivision;
- Will also be used to inform the design of new site specific developments at the site plan stage which are subject to site specific guidelines;
- Are intended to provide guidance for homeowners, designers, architects, developers, and landscape architects by outlining the Town's expectations for new development; and,
- As a planning tool minor changes or adjustments may be considered on a case-by-case basis without amendment to this document. However, major changes to the document are to be approved by Council.

1.2 Vision

Keswick will become a more complete, healthy and vibrant community, balancing its existing lakeside character with new development that meets the community's employment, shopping and entertainment needs, and provides more rental and affordable housing to support a diverse population. As Keswick evolves, new development and investment will prioritize the creation of a stronger sense of community, a well-connected and multimodal transportation network, the protection of natural areas and a commitment to environmental sustainability and resiliency.

1.3 Principles

To realize the vision for Keswick, the Guidelines shall support the Secondary Plan to achieve the following principles which will guide how the community will be designed and shaped, these include:

- To develop as a complete, healthy, attractive, safe, inclusive, and accessible community for the present and future residents of Keswick;
- To ensure a balance of low, medium and high density and mixed use development and intensification to meet the Town's growth targets and provide a full mix and range of housing options, including affordable and rental housing;
- To promote and strengthen community identity and cohesion by supporting mixed use community nodes, a high quality public realm and stronger connections and public access to the Lake Simcoe shoreline;
- To ensure that new development contributes to building resiliency, reflects efficient land use patterns and mitigates the impacts of climate change;
- To ensure that new development is integrated with existing land uses in a logical, orderly and efficient manner, and is coordinated with planning for transportation and municipal services;
- To ensure the provision of an accessible, efficient, connected and multimodal transportation network, that supports pedestrian-oriented environments and gives priority to the creation of complete streets and the provision of active transportation and transit infrastructure;
- To ensure that Keswick develops in a manner that promotes a competitive and adaptable economic environment that protects for future employment generating activities, encourages investment, provides a diversity of business and employment opportunities and incorporates high quality broadband connectivity; and,
- To protect the health and connectivity of Lake Simcoe and the Natural Heritage System and Parks Network, including public parks, open space and natural heritage and hydrologic features and their functions.



A pedestrian-oriented environment encourages walking and daily physical activity.

1.4 Urban Structure

Keswick's urban structure helps to organize the distribution of land use and identifies a number of key geographic components of the community that identify where growth will occur.

The Settlement Area Boundary

The Settlement Area Boundary establishes the jurisdictional extent of this Secondary Plan.

The Urban Service Area Boundary

Development on lands within the Urban Service Area Boundary are to be serviced by municipal sewer and water supply services. The Urban Service Area Boundary coincides with Settlement Area Boundary.

The Natural Heritage System and Parks Network

The Natural Heritage System and Parks Network provides an important structural element throughout Keswick, and includes lands designated Environmental Protection Area and Parks and Open Space.

The Delineated Built-up Area

The Delineated Built-up Area was defined in 2006 by the Province in order to identify those areas within the Settlement Area Boundary where the intensification target is to be measured.

The Designated Greenfield Area

The Designated Greenfield Area includes those areas within the Settlement Area Boundary that are outside of the Delineated Built-Up Area.

The Local Strategic Growth Area

The Local Strategic Growth Area are lands which have been identified as the focus for accommodating intensification, higher-density and a mix of uses in a more compact built form.

the 1990s, the number of people in the UK who are obese has increased by 50% (Health Survey for England 1995).

Obesity is a complex condition, with many causes. It is a result of an imbalance between energy intake and energy expenditure. The energy intake is determined by the amount of food and drink consumed, and the energy expenditure is determined by the amount of physical activity. The balance between these two factors is influenced by a number of factors, including genetics, environment, and lifestyle. The most common cause of obesity is a combination of these factors, with a genetic predisposition to obesity being the most significant. The environment, including the availability of food and the level of physical activity, also plays a role. Lifestyle factors, such as diet and exercise, are also important in the development of obesity.

Obesity is a major public health problem, with a significant impact on the health of the population. It is a risk factor for a number of chronic diseases, including heart disease, diabetes, and cancer. It also has a significant impact on the quality of life, with obese people often experiencing physical and psychological problems. The prevalence of obesity is increasing worldwide, and it is a major cause of mortality and morbidity.

The purpose of this study was to investigate the prevalence of obesity in the UK, and to identify the factors that are associated with obesity.

The study was a cross-sectional study, and it was conducted in the UK. The sample was a representative sample of the UK population, and it was divided into two groups: obese and non-obese. The obese group was defined as those people who had a body mass index (BMI) of 30 or greater, and the non-obese group was defined as those people who had a BMI of less than 30.

The data were collected from a number of sources, including the Health Survey for England, the National Diet and Nutrition Survey, and the National Child Development Study. The data were analysed using a number of statistical methods, including logistic regression.

The results of the study showed that the prevalence of obesity in the UK is increasing, and that it is a major public health problem. The study also identified a number of factors that are associated with obesity, including genetics, environment, and lifestyle. The study found that people who are obese are more likely to have a family history of obesity, and they are more likely to live in a high-calorie environment. They are also more likely to have a sedentary lifestyle, and they are more likely to eat a diet that is high in fat and sugar.

The study also found that people who are obese are more likely to have a number of chronic diseases, including heart disease, diabetes, and cancer.

The study has a number of strengths, including the fact that it was a large, representative study. It also had a number of limitations, including the fact that it was a cross-sectional study, and it did not take account of changes in weight over time.

The study has a number of implications for public health. It suggests that there is a need to take action to reduce the prevalence of obesity, and that this should be done by addressing the factors that are associated with obesity. This includes promoting a healthy diet and lifestyle, and creating an environment that is supportive of healthy living.

The study also has a number of implications for research. It suggests that there is a need to conduct further research into the causes of obesity, and into the factors that are associated with obesity.

The study was funded by the Medical Research Council, and it was conducted by the Medical Research Council Social, Genetic, and Developmental Psychiatry Centre. The study was approved by the local research ethics committee.

The authors would like to thank the following people for their contribution to the study: the participants, the research assistants, and the staff of the Medical Research Council Social, Genetic, and Developmental Psychiatry Centre.

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2. THE PUBLIC REALM

As the population of Keswick continues to grow, it will require a public realm that continues to support the needs of its existing residents, new residents, and visitors. The public realm comprises public roads, lanes, parks and open spaces, natural heritage features and their associated buffers, stormwater management facilities, and the public use activity areas of other public lands and private development sites and buildings. Moving people efficiently through the community and providing a variety of public spaces for socializing and recreation are key priorities that form the basis of the public realm guidelines.

The guidelines will be considered when municipal initiatives or private development applications impacts elements of the public realm.

2.1 General Guidelines

1. Encourage opportunities for vibrant, diverse and pedestrian-oriented urban environments that provide for public safety, changing experiences, social engagement, and meaningful destinations.
2. Provide for mixed-use neighbourhoods that are walkable with connected public gathering places, where opportunities for social interaction are increased and services can be provided within easy walking or cycling distance or by use of public transit.
3. Promote internal connectivity and multiple connections to the community at large, taking into account the existing and proposed urban structure of adjacent and adjoining areas.
4. Provide for an interconnected network of sidewalks, bicycle routes, transit, and multi-use trails ensuring proper integration with surrounding neighbourhoods and a variety of destinations, allowing for continuous movement throughout the community.



Street trees provide shade over the sidewalk.



An enhanced public realm with plantings, decorative paving, and wood awning.

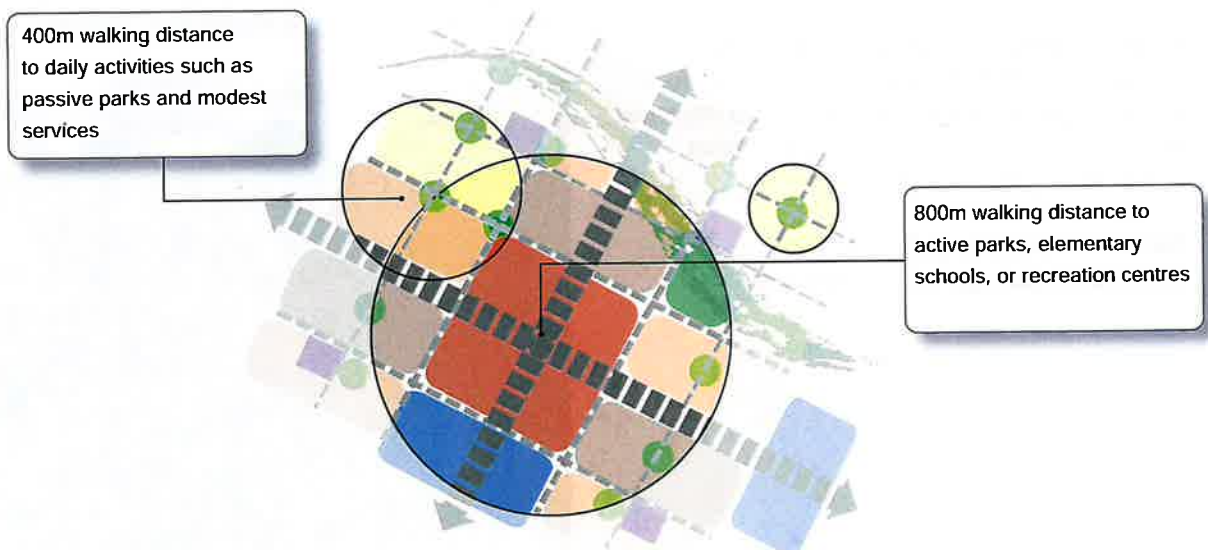
5. Ensure a typical walking distance of 400 metres (5 minute walk) to daily activities, such as passive parks, and modest services, or 800 metres (10 minute walk) to active parks, elementary schools, or recreation centres.
6. Design the street layout to ensure efficient walking routes to schools, centres, transit, and other key destinations. Continuous sidewalks, or equivalent provisions for walking should be provided on both sides of the road, where necessary.
7. Implement traffic calming measures in high activity areas, such as those around Urban Centres and Neighbourhood Centres, that may include on-street parking, reduced lane widths, public laneways, woonerfs, or home zones (i.e. the speed limit is under 15km/hr and vehicles must yield to pedestrians and cyclists), raised intersections, medians, curb bulb-outs, and/or traffic circles to reduce vehicular traffic speeds and to ensure safe walking and cycling environments.
8. Provide neighbourhood permeability by designing blocks to be between 150 to 250 metres in length to promote active transportation, discourage excessive driver speed, and disperse traffic movements.

2.2 Guidelines for Roads

Refer to Appendix A of the Town of Georgina Development Design Criteria for typical road cross sections in the Town. Regional Roads are under the jurisdiction of York Region and reference shall be made to those standards. The specific technical details of the road cross-sections (i.e., engineering standards) will be determined through the appropriate design review process.

2.2.1 General Guidelines

1. Design a permeable network of roads with strong links and route choices between urban and neighbourhood centres, mixed use corridors, and neighbourhoods.
2. Design the road and block pattern to emphasize connections and walkability both internally and with surrounding neighbourhoods, through a grid or modified grid pattern discouraging cul-de-sacs, p-loops and crescents, except where necessary due to grading and topography.
3. Avoid back-lotting or reverse lot frontages where feasible, and is not considered unless demonstrated to be the only option.



Locating services and amenities within walking distance supports daily physical activity and reduces the reliance on the private automobile.

4. For blocks with grade-related residential units, encourage street and block alignments within 15-degrees of geographic east-west to maximize passive solar orientation of buildings.
5. Respond to natural heritage features in planning the road network and provide public streets along the edges of natural heritage features.
6. The design of all roads shall include defined and, wherever possible, continuous zones for plantings, street furnishings, utilities, pedestrian sidewalks, bicycle lanes, and vehicular pavements.
7. Construct all sidewalks to municipal standards and accommodate on all street types, generally on both sides of the street, to facilitate pedestrian and bicycle circulation.
8. Plant street trees to create and enhance the urban tree canopy while providing shade over sidewalks.
9. Road design for Arterial and Collector Roads is encouraged to include a raised centre median, with a minimum width of 4.0 metres, and will include trees, shrubs, and ground covers. Centre medians should be used to signify a gateway or entrance to a centre or a neighbourhood.
10. Introduce green infrastructure, such as bioswales, within the public right-of-way to enhance ground water infiltration and improve water quality as part of a comprehensive water management plan.
11. Terminate roads at public facilities or landmark buildings, where possible.



Large canopy trees provide shade over the sidewalk.



Planted median used to signify an entrance to an urban centre.



Local Road with a bioswale in the right-of-way to assist with run-off and infiltration.

2.2.2 Regional Roads

Regional Roads are primarily envisioned as providing routes for vehicles, pedestrians, and cyclists through Keswick and across Georgina.

1. Regional Roads, such as Woodbine Avenue, Old Homestead Road, and Ravenshoe Road are primarily designed to function as transportation corridors providing routes for vehicles, pedestrians, and cyclists through Keswick and across Georgina.
2. Regional Roads are intended to accommodate large volumes of traffic and generally have a right-of-way-width of up to 36.0 metres.
3. Regional Roads will be designed to Regional standards and access to individual properties may be permitted although the number, design, and location of access points will be controlled.

2.2.3 Collector Roads

Collector Roads connect to Arterial Roads and provide primary connections to Local Roads.

1. Collector Roads generally have a right-of-way width of 23.0 to 30.0 metres.
2. Collector Roads may include optional 3.0 metre on-street parking on both sides of the road.
3. Collector Roads have boulevards on both sides of the pavement and accommodate a grass verge with street trees and minimum 1.5 metre sidewalks on both sides. Provide separated or shared space for cyclists with or without separation from traffic lanes.
4. Transit facilities may be accommodated on all Collector Roads.
5. Limit individual direct access to any development site to minimize disruptions to traffic flow and to maximize safety and the attractiveness of the road.
6. Ensure buildings that abut Collector Roads present a facade with architectural detailing and landscape features that address the road frontage.



Collector Road lined with rear lane live-work units and lay-by parking.

2.2.4 Local Roads

Local Roads provide the fine-grain transportation network for the community, connecting to Collector Roads and linking with public spaces.

1. Local Roads generally have a right-of-way width of 18.0 to 20.0 metres.
2. The road surface will be a maximum of 8.5 metres, including a parking lane on one side of the road, that could alternate to both sides of the road.
3. Local Roads have boulevards on both sides of the pavement and accommodate a grass verge with street trees and a minimum 1.5 metre sidewalk on either the north or east side of the road.
4. Accommodate parking on Local Roads adjacent to centres, corridors, and main streets. On-street parking may be accommodated on both sides of the road.
5. Individual direct access onto Local Roads is permitted.
6. Ensure buildings that abut Local Roads present a facade with architectural detailing and landscape features that address the road frontage.



A Local Road with street tree planting.

2.2.5 Main Streets

The Queensway provides a north-south route through Keswick, including all three Urban Centres - the Glenwoods Urban Centre, the Maskinonge Urban Centre, and the Uptown Keswick Urban Centre.

The character of The Queensway within the Urban Centres is that of a pedestrian scale Main Street with small scale street-oriented built form. These centres have an urban character and serve the needs of the existing neighbourhoods that surround them.

1. Design Main Streets to include the following:
 - a. Traffic calming measures at intersections;
 - b. Tightly spaced street trees in planters and continuous street pits;
 - c. Wider sidewalks with a minimum width of 1.8 to 3.0 metres in high pedestrian areas in Urban Centres particularly where retail is provided along the street;
 - d. Coordinated street furniture, lighting, and signage; and,
 - e. On-street and lay-by parking areas.



A Main Street with strong street presence through street parking, wide sidewalks, special paving, street trees, landscaping, and street furniture.

2.2.6 Window Roads

Window Roads are proposed in particular situations to avoid residential reverse lotting and frontages directly along arterial roads. Window Roads can be Collector or Local Roads that are typically single-loaded and are parallel to adjacent Regional Roads, and can be used along natural heritage features, to provide the opportunity to enhance the character of the community.

Window Roads are intended to provide a safe and comfortable pedestrian experience with allowances for driveway access from the road.

1. Window Roads generally have a 18.0 metre right-of-way with one lane in each direction, on-street parking and a 1.5 metre wide sidewalk on the residential side. A second sidewalk or, where feasible, multi-use trail will be integrated into the right-of-way of the adjoining Regional Road with direct pedestrian connections to the window street.
2. The boulevard treatment shall consist of street trees on the dwelling side boulevard and trees with buffer planting and low decorative fencing within a grass boulevard adjacent to the Regional Road boulevard. Landscaping of Window Roads shall be consistent in design.



Window Road with street trees and buffer plantings on the boulevard adjacent to a Regional Road.

3. Design of Window Road treatments shall take into consideration noise attenuation, grading issues, the need for headlight screening and safe pedestrian access into the neighbourhood.
4. Highlight pedestrian access points with decorative hard or soft landscaping.
5. Single-loaded Collector or Local Roads, adjacent to natural features will provide unobstructed views to the natural areas and, where appropriate, accommodate a trail along the edge of the street for active uses, such as walking and cycling.

2.2.7 Private Roads

The following guidelines apply to new roads which will not be owned or maintained by the Town and which facilitate access to new multi-unit residential, commercial, or mixed-use developments on private properties. The guidelines do not apply to historic private roads which facilitate access to existing development.

1. Private Roads generally have a right-of-way width of 9.0 metres with a minimum paved surface width of 6.0 metres.
2. Provide a minimum 1.5 metre landscaped utility corridor on either side of the Private Road.
3. Sidewalks are required on at least one side of a Private Road, and may be located within the utility corridor.
4. Consider the use of permeable or porous materials in areas where sufficient drainage exists.

2.2.8 Municipal Lanes

Rear Lanes provide significant benefits such as enabling continuous street tree planting and creating safer pedestrian environments through the removal of driveways from the street edge. Lanes may be used in key locations where private access along prominent streets should be minimized.

1. Provide Lanes along Regional or Collector Roads where garages and front driveways are not permitted, and in areas where driveways will detract from the character of a specific location, such as along a retail street.
2. Lanes generally have a right-of-way width of 8.0 metres with a minimum paved surface of 6.0 metres and a 1.0-metre utility corridor on either side of the Lane.
3. Consider the use of permeable or porous materials in areas where sufficient drainage exists
4. Any garage should be set back a minimum of 0.6 metres from the Lane right-of-way.
5. The desirable Lane length is a maximum of 150 metres to be consistent with fire hydrant spacing on road connections.
6. Consider Lanes in the rear of residential units facing Neighbourhood Parks.
7. Provide landscape areas in Lanes where possible to enhance lane appeal and promote their use as gathering and playing areas.



Lane with landscaping to enhance the visual appeal.



Lane with parking pad and private outdoor amenity space over the garage.



Decorative paving and wider sidewalks in Urban Centres.



Sidewalks with street trees to provide shade.



Planters and benches concentrated at key intersections in Urban Centres.

2.2.9 Streetscape Elements

Sidewalks

1. Ensure sidewalks are continuous throughout the community and constitute an integral part of the pedestrian system to promote active transportation. Design sidewalks as follows:
 - 1.5 metres on Local Roads;
 - 1.5 to 2.0 metres on Collector and Regional Roads; and,
 - 1.8 to 3.0 metres in high pedestrian areas in Urban Centres particularly where retail is provided along the street.

In all cases, provide sufficient space for street furnishings, public utilities, lighting, tree plantings, and transit shelters.

2. In order to accommodate the needs of persons with disabilities and the elderly, design sidewalks to applicable municipal standards.

Street Trees and Planting

1. Plant street trees to contribute to the urban tree canopy, to incorporate a buffer to separate the pedestrian from moving vehicles, and to create a canopy and shade over sidewalks in order to enhance pedestrian comfort and safety.
2. Where appropriate, use drought resistant and salt tolerant landscaping within medians to visually soften the pedestrian environment.
3. Utilize a comprehensive planting and soils strategy based upon species diversity, resiliency, and urban tolerance.
4. Consider a diversity of native tree species along each street.

Street Furniture

1. Concentrate street furniture in areas with the highest pedestrian traffic, such as urban centres, key intersections, and parks.

2. Street furniture should include elements such as pedestrian scaled lighting, benches, bicycle racks, and trash bins and where possible, should be manufactured from recycled material.

Wayfinding Signage

1. Develop comprehensive wayfinding strategies to include directional signage and mapping at key locations, such as mixed-use nodes, neighbourhood centres, and key intersections. Key destinations also include the waterfront, parks, public transit stations, community/recreation centres, and off-road trails. The purpose of wayfinding signage is take residents or visitors from one location to a desired destination by walking or cycling.
2. Create wayfinding signage from high quality materials and ensure it is easy to understand, highly visible, visually interesting, and aid pedestrians and drivers in navigating the area, especially at night.

Pedestrian Crossings

1. In order to promote walkability and a pedestrian-focused environment provide formal pedestrian crossings at every four-way intersection in high pedestrian areas.
2. Provide signalized pedestrian crosswalks at locations where important destinations or significant walking traffic is anticipated, such as near retail shops, schools, and places of worship.

3. Ensure pedestrian crossings have a minimum width of 3.0 metres, are continuous, and connect to adjacent sidewalks.
4. To enhance the visibility and quality of pedestrian crossings, utilize distinctive feature paving to minimize the conflict between vehicles and pedestrians. This could include the use of alternative paving markings or materials. At a minimum, identify crossings with distinctive painted lines.
5. Minimize the height of curb cuts to facilitate wheel-chair and stroller usage in high pedestrian areas.

On-Street Parking

On-street parking functions as a traffic calming device by slowing traffic and acts as a safety buffer by separating the pedestrian realm from vehicles.

1. Provide parking on the same side of the street as the sidewalk for Local and Collector Roads.
2. Provide lay-by parking or on-street parking bays on both sides of the road in the Urban Centres.



Lay-by parking in front of retail in an Urban Centre.



Integrate the natural heritage system with the community.



Incorporate recreational opportunities such as cycling trails within the natural heritage system to encourage physical activity.

2.3 The Natural Heritage System and Parks Network

The Natural Heritage System and Parks Network is a major functional, structural, and aesthetic component of Keswick and should be designed to provide a fair distribution of amenity spaces for a range of users.

The natural environment, urban forest, parks, open space, and trail systems are essential components of a healthy, sustainable community ensuring residents have convenient access to a connected and diverse range of recreational opportunities.

2.3.1 Natural Heritage System

The Natural Heritage System contributes to the community's character and is a key structural element of Keswick.

The following guidelines aim to protect, restore and enhance the Natural Heritage System, while mitigating any existing or potential negative impacts due to urbanization and development. They ensure natural heritage features are woven into the fabric of the community, providing important ecological functions, enhancements to community character through views, and recreational opportunities, where appropriate.

General Guidelines

1. As opportunities arise, connect and integrate the Natural Heritage System with the Parks Network and the local and regional trail systems to buffer and expand natural heritage features and functions, ensuring ecological systems are not interrupted.
2. Integrate the Natural Heritage System as a key structural element in each neighbourhood's design by providing for a range of development interfaces that create opportunities for public vistas and connections to the Natural Heritage System (e.g. terminal views at the end of prominent streets).
3. Incorporate recreational opportunities such as trails within the Natural Heritage System to encourage physical activity, where negative impacts will not occur.

4. Provide frequent access points to, and significant street frontage along, NHS areas to promote views, where appropriate.
5. Provide naturalization planting and restoration to enhance the urban ecology and function of natural heritage features and their adjacent lands.

Woodlands

1. Preserve and expand existing tree cover to connect and buffer protected woodlands and other natural areas and to mitigate heat island impacts.
2. Provide opportunities for naturalized plantings and landscape restoration to enhance and help to establish local ecological features.
3. Prevent direct access from private properties backing onto woodlands.
4. Locate pedestrian access to trails where there is no long term impact to the existing vegetation and wildlife communities.
5. Lighting within woodlands is discouraged to protect ecological features and functions of the natural setting.

Urban Forest

Trees provide ecological services that benefit human and environmental health, such as reducing heat island effect, sequestering greenhouse gases, providing shade in the summer, separating pedestrians from vehicular traffic, and contributing to more appealing sidewalks and streets.

1. Provide robust species selection to anticipate climate change conditions and operational constraints.
2. Provide street trees on both sides of the road in the public right-of-way.
3. Encourage a diversity of tree species along each road, native to the Municipality and Region, non-invasive, drought and salt tolerant, and low maintenance.



Opportunities for walking trails through the natural heritage system.



Trails to the natural heritage system should be connected to the public sidewalk.



Street tree canopy contributes to the urban forest.



Street tree canopy in a planting a trench with low shrubs.



Houses fronting the park and trail system.



Houses overlooking a playground.

4. Plant a double row of trees in key areas, such as adjacent to parks and where a wider boulevard exists.
5. Encourage the delivery of alternative planting strategies along high-pedestrian areas such as silva-cells, sufficient soil medium, continuous planting trenches, etc., to sustain long-term growth and healthier tree life.

2.3.2 Parks Network

A Parks Network is connected to the natural environment, and throughout the community, and provides for a variety of open spaces, parks, and recreation facilities to support opportunities for improved public health. Convenient access to these amenities encourages residents to walk and cycle, in addition to providing places for gathering, socializing, and active and passive recreation.

1. Incorporate the following Crime Prevention through Environmental Design principles into the design of parks:
 - Ensure clear views into and out of surrounding areas, including:
 - Adequate lighting to illuminate paths;
 - Buildings oriented to overlook public spaces. Playgrounds should be highly visible to public streets and/or houses to enhance safety;
 - Proper site design and signage for ease of access and egress; and,
 - Program parks with a mix of activities for constant use of the space.
2. Ensure new trees and landscaping within parks are native plant materials, and where possible, salvaged from the site or the local area.
3. Provide lighting that is Dark Sky/Nighttime Friendly compliant.
4. Incorporate LED lighting or solar powered lighting for natural trails, park pathways, and other public spaces to reduce electric energy supply in the public realm.

5. Consider public art as focal points in open spaces to reflect the cultural heritage of the location. Public art can include memorials, sculptures, water features, or individual installations at visually prominent sites.
6. Locate and design parks and open spaces to support, complement, and buffer the Natural Heritage System

2.3.3 Community Parks

1. Community Parks are intended to primarily serve the broader community and shall generally be between 4.0 to 8.0 hectares in size.
2. Locate Community Parks at the intersection of arterial or collector roads, with significant frontage for easy access to the surrounding neighbourhoods and/or a defined service area.
3. Consider locating Community Parks adjacent to secondary schools to allow for shared use of facilities and parking.
4. Link Community Parks to the Natural Heritage System and pedestrian and bicycle trails, where feasible.
5. Direct lighting for sports fields within Community Parks away from the Natural Heritage System and designed to minimize disturbance to adjacent properties.
6. Consider incorporating community facilities such as community centres, recreation centres, and/or arenas in a Community Park.
7. Community centres or recreation centres in Community Parks will be designed such that the building addresses the principal street edge and provides sidewalk connections to adjacent transit stops to create a pedestrian-oriented public edge. Multi-storey buildings are encouraged to make efficient use of land and contribute to a compact built form.
8. Consider community gardens in Community Parks to encourage social interaction and provide access to locally grown produce.



Public Art serves as a defining feature for a public park.



Pathways in a park encourage safe and efficient pedestrian circulation.



Parks located adjacent to the natural heritage system.

2.3.4 Neighbourhood Parks

1. Neighbourhood Parks are intended to primarily serve local residents within a 10 minute walk (approximately 800 metres) and shall generally be between 1.5 to 2.0 hectares in size
2. Plan Neighbourhood Parks as focal points of neighbourhoods, preferably centrally located at the terminus of a major street or at the corner of a main intersection, and within walking distance of schools and other community amenities and destinations.
3. Ensure Neighbourhood Parks have significant frontage on adjacent streets to promote views and reinforce their focal nature. Street frontage shall not be less than 30% of the park perimeter.
4. Locate Neighbourhood Parks adjacent to school sites, where appropriate, to allow for shared amenities, such as parking lots and recreational fields. Recreational fields shall be constructed using appropriate durable turf treatments to minimize maintenance and extend the life of the field.
5. Provide on-street parking adjacent to the park to create a barrier edge. Parking can be either lay-by parking or on-street, depending on the scale of the park and the nature of the streetscape.
6. The backing of residential lots onto Neighbourhood Parks is discouraged.
7. Coordinate the design of park structures, such as gazebos, with other neighbourhood elements such as transit stops and community mail boxes.
8. Include a range of active and passive recreation, such as playgrounds, courts, walkways, seating, planting areas, and/or natural or cultural features in Neighbourhood Parks.
9. Provide bicycle parking in Neighbourhood Parks. Bike racks should be accessible and conveniently located adjacent to play areas and park entrances, with hard surfaces under the bike rack.



Residential fronting onto the Neighbourhood Park, with areas for seating and shade.

2.3.5 Village Greens

1. Village Greens are intended to primarily serve higher density areas with a more urban character and/or where the provision of a neighbourhood park is not practical or feasible.
2. Village Greens may be less than 0.5 hectares in size and are generally located within urban centres and mixed use corridors and planned as active use spaces with playgrounds, seating areas, and walking paths.
3. Ensure Village Greens have street frontage on at least two public streets, three is encouraged.
4. Locate Village Greens to achieve significant public exposure and access. Urban design options include surrounding the park with streets or fronting dwellings directly onto the Village Green to create visually attractive 'edges' to these spaces and eyes-on the park.
5. Design Village Greens to enhance the character of the adjacent land uses, provide spaces for gathering, areas for seating, plantings to provide shade over seating areas or along pathways, and active use spaces.
6. Ensure Village Greens complement and enhance the surrounding public realm by integrating the design of landscape treatments such as built form features, site furniture, and landscape elements with adjacent streetscapes and public spaces.
7. Connect formalized paths within Village Greens should connect to pedestrian sidewalks and trails.
8. To ensure utilization and presence, the Village Green should be fronted by animated uses with a consistent building setback and a high level of transparency. These would be in high pedestrian areas with uses such as restaurants and cafes, preferably with some outdoor seating areas.



Active recreation through the use of playgrounds.



Residential surrounding the Village Green.



Provide areas of shade and seating in Village Greens.



Larger urban Village Greens with distinctive and high quality paving can be used to hold large-scale, occasional events, such as a farmers market.



Pedestrian linkage that connects to the Natural Heritage System.



Open space linkages support active transportation and should connect to the natural heritage system.

9. When located in an Urban Centre, use distinctive, high quality paving treatments for the Village Green, with consideration given to extending the paving treatment onto the street to give the space further prominence. This additional area would delineate an extended space that could be occasionally utilized for large-scale events such as a farmers market or festival.

2.3.6 Open Space Linkages

1. Open space linkages are linear parks that support active transportation, improve community connectivity, and link parks, open space areas, and the Natural Heritage System. Support community connectivity by providing frequent openings and access points along open space linkages.
2. Utilize utility corridors, abandoned railway lines, or easements for open space linkages to contribute to a continuous linear open space system.
3. Open space linkages may include multi-purpose trails intended for passive recreational purposes such as walking, jogging, cycling, and mobility aid riding. Design multi-use trails to accommodate a range of users and abilities and to be barrier-free, where appropriate.
4. Refer to Section 2.4.1 for additional guidelines for the pedestrian and cycling system.

2.3.7 Urban Agriculture

Urban agriculture provides the opportunity for an alternative use of green space and as a transition in land uses such as community gardens and traditional farm areas at community peripheries.

1. Promote initiatives such as sustainable food production practices as a component of a new development. Development plans and building designs shall incorporate opportunities for local food production through:
 - Community gardens;
 - Edible landscapes;

- Small scale food processing (i.e., community kitchens, food co-ops, community food centres);
- Food-related home occupations/ industries;
- Small and medium scaled food retailers; and,
- Local market space (i.e., a farmer's market).

2. Incorporate urban agriculture as part of a neighbourhood's character and open space system, while also providing a transitional use between the natural and built environments. Measures to protect natural features must be considered.

2.3.8 Gateways

1. Design gateways to identify the intersection as an entry point into the community.
2. Incorporate gateway features, such as community signage, low walls, fencing or enhanced landscape treatment in the design of entry road intersections. Coordinate the design and materials with adjacent structures.
3. Include a planted centre median and other design features for gateways and entry roads into the community to signify their importance.
4. Utilize distinctive surface treatment for pedestrian crossings, including wider sidewalks, and connections to bus shelters at gateway intersections.

2.3.9 Views and Vistas

Enhancing the views of important community elements for residents can assist in the creation of a sense of place. The best way to achieve those views is through the orientation of streets and buildings.

1. Orient streets to maximize views to the natural heritage system and Lake Simcoe. These views are an opportunity to reinforce these natural elements as landmark features.



Urban agriculture as a component of the parks system.



Streetscape features to enhance a gateway entrance.



Natural heritage features should be located at the terminus of view corridors.



View across Cook's Bay.



Naturalized edge of a stormwater management pond.



Ponds should blend with the natural landscape.

2. Existing natural features should form the basis for directing views.
3. Protect significant views through the location and configuration of open space opportunities.
4. Where possible, site community buildings such as schools, churches, and community facilities as view terminations.
5. Design buildings that terminate views as special landmark buildings.

2.3.10 Stormwater Management Facilities

Stormwater management facilities should be developed in a manner that will yield the greatest environmental and amenity benefit to the neighbourhood, which can be achieved first through reducing stormwater run-off and flow to the ponds, and secondly, through the design and landscaping of the pond. These facilities promote sustainability by providing habitat, enhancing ecosystem structure and resilience, and managing stormwater on site.

1. Design stormwater management facilities as major open space features that provide passive recreational and educational opportunities, while augmenting the extent of the community's open spaces and associated microclimatic benefits.
2. Enhance views and access to ponds by designing a portion of the pond to be bounded by either streets and/or open space.
3. Pond Design and Landscaping:
 - a. Locate ponds off line and as buffering to environmental features;
 - b. Landscape ponds to contribute to the urban tree canopy, add to the natural features of the community, and support wildlife habitat;
 - c. In addition to functional objectives related to flow moderation and water quality, design ponds as key focal/visual features within the community; and,

- d. Design ponds as part of the overall pedestrian and trail system with view points and interpretive signage. Surround ponds with public walking or cycling trails and extend along stormwater channels.
4. Fencing of the entire perimeter of stormwater management ponds is discouraged, except where necessary along steep slopes, or the rear or flankage of residential property lines. Install 1.8 metre high black-vinyl-coated chainlink fencing along the property line where the stormwater management facility block abuts private property. It should be continuous with no gates permitted.
5. Fencing is not required along the property line where a stormwater management facility abuts a public park, open space, natural area, or road right-of-way.
6. Consider on-site treatment of stormwater through the use of green infrastructure such as bioswales, at source infiltration, and permeable pavement.
7. Design stormwater management facilities to blend with the natural landscape. Where feasible, conceal inlet and outlet structures using a combination of planting, grading, and natural stone.
8. Ensure the edges of ponds abutting natural heritage features remain naturalized.
9. Install signage at prominent locations along the road frontage or in an appropriate location along the interface between the pond block and the adjacent open space to ensure it is highly visible to the public. The purpose of signage is to identify the site as a stormwater management facility and raise public awareness of the functional aspects and related potential hazards of the facility.
10. Coordinate landscape components such as look-outs, seating areas, fountains and gazebos to complement the overall character of the pond.



Formal hardscaped paths, seating, and playgrounds are located around the perimeter of the pond.



Permeable pavers to support on site stormwater infiltration.



Ponds incorporated as an amenity, with trails and lookouts.



Pedestrian walkways through parks.



Clearly marked cycling lanes painted on the road.



Bicycle parking at transit stops.

2.4 Active Transportation

2.4.1 Pedestrian and Cycling Network

Encourage active transportation and support physical activity through the provision of a linked system of trails that ensure residents have increased access and mobility options to local destinations for work and play. The following guidelines should be considered in relation to the pedestrian and cycling network.

1. Create a continuous and diverse active transportation network of inter-connected pedestrian and cycling routes and trails, walkways, sidewalks, and bicycle lanes that link the community with surrounding neighbourhoods, integrate with existing and future public transit infrastructure, and connect to sidewalks and the open space system.
2. Encourage safe routes to schools by providing a network of connected local streets with inherent traffic calming measures. To ensure safe use by young pedestrians and cyclists, such measures may include reduced lane widths, raised intersections, slower vehicle speeds, on-street parking, and crosswalks.
3. Develop a cycling network that includes bike lanes and off-road cycling or multi-use trails that connect to existing bike lanes and trails. The standards of the York Region Pedestrian and Cycling Master Plan should be followed where appropriate.
4. Design shared off-street pedestrian and bicycle paths based on the requirements of the route. Provide a continuous, linked, legible, and clearly marked system of trails throughout the community as part of the open space network. Pedestrian and cycling lanes should be painted along multi-use trails or clearly identified by other means to minimize pedestrian and cycling conflicts.
5. Provide, where feasible, clearly marked bike lanes on Collector Roads and consider further separation by including a painted buffer.
6. Wherever possible, connect pedestrian and cycling routes with transit stops.

7. Design trails to be barrier-free and accommodate a range of users and abilities, where appropriate. Where possible, slopes, should be under 5%. Provide curb-cuts and other safety measures to improve access at road crossings.
8. Trails must be clearly signed regarding permitted uses and speed. Provide wayfinding signage and trail markers throughout the trail network.
9. Incorporate interpretive signage on trails located in proximity to significant natural heritage features or adjacent to stormwater management facilities to educate and promote stewardship initiatives that will protect and enhance the features and functions of the natural landscape.
10. Consider special treatments at trail head entrances including features such as landscaping, benches, natural or built shade structures, decorative paving pattern, interpretive or directional signage, or wider pathway widths.
11. Design trails to minimize and mitigate impacts on natural heritage features. Consider the use of low impact materials such as wood chips, limestone screenings, or permeable materials for trail construction in areas where sufficient drainage exists.
12. Trails with asphalt surfaces may be incorporated into the trails system to address accessibility and active transportation needs.
13. Provide lighting for pedestrian safety along primary connecting trails. Lighting is not acceptable in natural heritage features.
14. Avoid constructing trails in low-lying areas. Where they do occur, implement boardwalks, bridges, culverts, and swales as support systems.
15. Along trails abutting natural features use native, non-invasive species that can contribute to the urban tree canopy and shade trails.



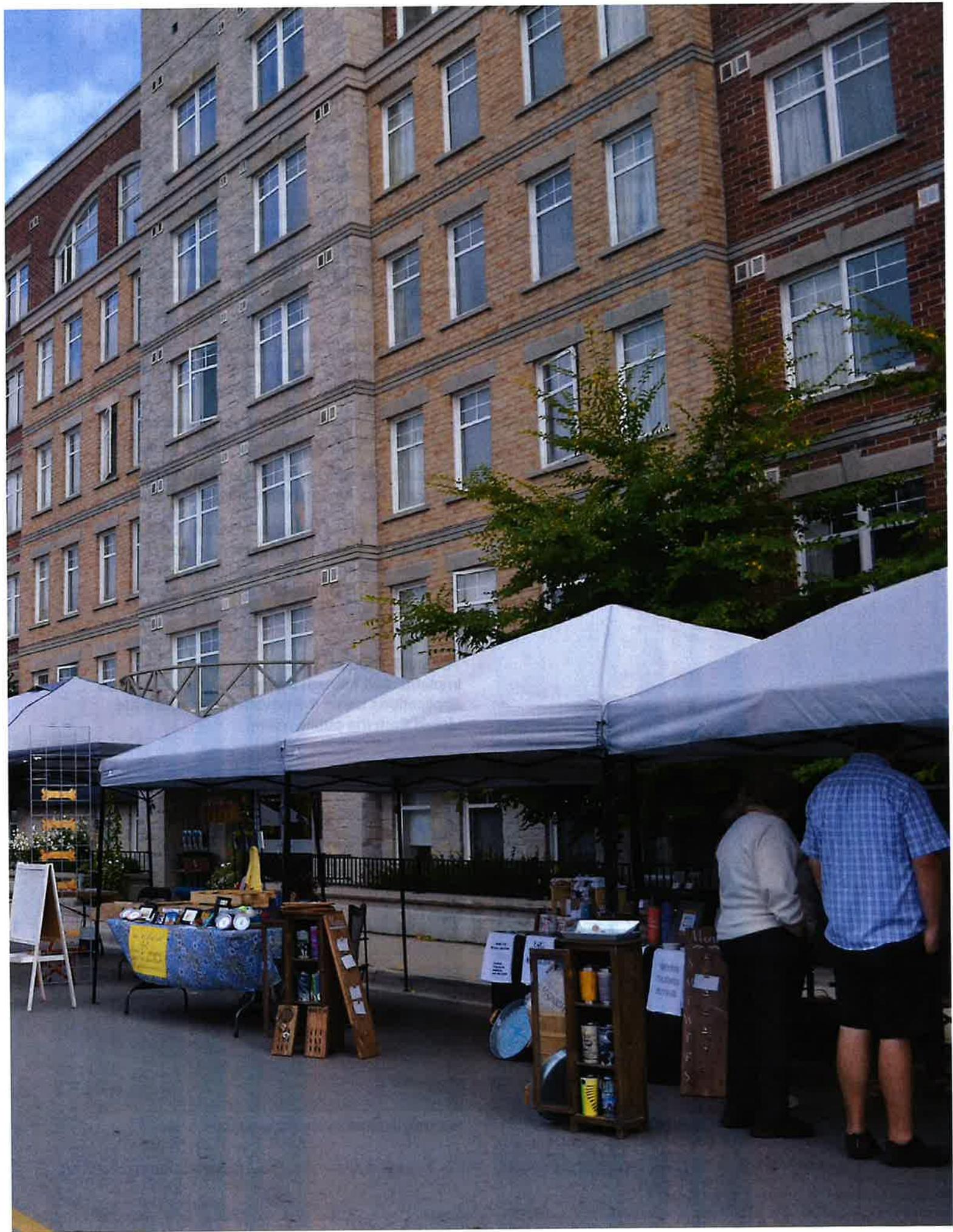
Trail designed to accommodate a range of users.



Wayfinding signage at a trail head.



Trail designed using permeable materials.



3. THE PRIVATE REALM

The private realm within Keswick is comprised of the built form and site design within development blocks and their relationship to adjacent open spaces and roads. The residential, institutional, commercial, and mixed-use buildings within a community contribute to its character and can assist in further defining and complementing the public realm. The design of development within the private realm shall be based upon principles of place-making and design excellence.

Place-Making

Place-making involves a multi-faceted approach to the planning, design, and management of private development. Place-making involves creating quality spaces that recognize and enhance a community's unique aspects. The unique aspects of a neighbourhood contribute to creating a recognizable and defined character which are collectively experienced from the viewpoint of the public realm.

Design Excellence

Good urban design practices will promote excellence in the design of the private realm. While the specifics of each development proposal may vary, the overall objectives will remain the same throughout Keswick. These objectives include:

- Creating distinctive, appealing, and pedestrian friendly streetscapes through attention to building design and detailing;
- Ensuring appropriate massing, materials, building siting, and design compatibility; and.
- Identifying enhanced design requirements for priority lots having highly visible elevations.

This section of the document provides guidance for the design of built form in the private realm and how it should address roads and open spaces. These Guidelines are to be read in conjunction with the policies of the Keswick Secondary Plan.

The guidelines will be considered and implemented through the review of development applications within the private realm which are visible from the public realm.



Townhouse units fronting onto a pedestrian walkway.



Four storey apartment building with an articulated facade.



Example of a Mid-rise building.



Multi-storey mixed use buildings with activity at the ground level.



Public art in an urban centre acts as a focal point.

3.1 General Guidelines for all Development

All development shall ensure excellence in design, be designed to achieve a high degree of environmental sustainability and demonstrate high quality architectural detailing, in accordance with the following guidelines.

3.1.1 Urban Centres and Mixed-Use Corridors

1. All development within the three Urban Centres: Glenwoods Urban Centre, Maskinonge Urban Centre, and Uptown Keswick Urban Centre; and the two Mixed-Use Corridors: The Queensway and Woodbine Avenue, shall be compatible with adjacent uses. Any proposed new development will have regard for adjacent low-rise residential built forms, with respect to existing building mass, height, setbacks, orientation, landscaping, and visual impact.
2. All development shall demonstrate design excellence and compatibility with its surrounding context. Architectural detailing, landscape treatments, colour, and building materials shall be representative of the highest quality possible.
3. Locate higher density forms of development, mixed uses, and live-work units in Urban Centres, along Mixed-Use Corridors, and at gateways to create areas of community focus.
4. Promote multi-storey buildings that create an urban street condition with building façade proportions that contribute to a comfortable pedestrian experience.
5. Encourage public art in Urban Centres and Mixed-Use Corridors, and as focal points in open spaces to reflect the character of the location. Public art can include memorials, sculptures, water features, murals or individual installations at visually prominent sites.

6. Ensure all buildings abutting The Queensway include the use of quality materials, articulated façades with window displays, and high activity uses at-grade, such as retail stores and restaurants, to animate the streetscape.
7. Ensure access from sidewalks and public open space areas to primary building entrances is convenient and direct, with minimum changes in grade. Entrances shall be accessible to people who are mobility challenged.
8. Ensure primary entrances to buildings are clearly visible, include lighting, and are located on a public road or onto public open spaces in order to support public transit and for reasons of public safety and convenience. Secondary doors, such as those that face a parking area, emergency exits, or service doors should be designed to blend in with the building façade.

3.1.2 Neighbourhoods

1. New residential blocks shall contain a mix of dwelling types with a variety of elevations to ensure a diverse housing stock and to avoid a homogeneous streetscape.
2. Residential lots should be generally simple and rectilinear however, variations are permitted if deemed necessary based on environmental features, topography, property boundaries, or other limiting features.
3. Ensure appropriate transitions in terms of height and massing between buildings of different densities, particularly if they belong in the same block.
4. Townhouse built forms may be used to transition from low-rise neighbourhoods to denser areas with mid- and high-rise buildings.
5. Rear Lane vehicular access is encouraged for grade related residential development to provide for a more pedestrian-friendly streetscape.



Mix and variety of housing types.



Example of Live-work units.



Example of a single detached unit.

3.1.3 Cultural Heritage

The development and sensitive integration of new buildings and landscapes shall ensure that adjacent cultural heritage resources are respected, protected, and enhanced.

1. Encourage the adaptive reuse of heritage structures, where appropriate. Relocation is encouraged only where maintaining the original location is not feasible.
2. The retention, restoration, and adaptive reuse of existing heritage buildings in their original locations is a priority to provide a tangible example of the cultural heritage of the area.
3. Where cultural heritage resources have been identified, new development shall provide a transition in lot sizes, setbacks, massing, and grading that complements the cultural heritage resource.
4. For new buildings located adjacent to cultural heritage resources, ensure that designs are complementary to existing heritage buildings with respect to colour and material palettes.



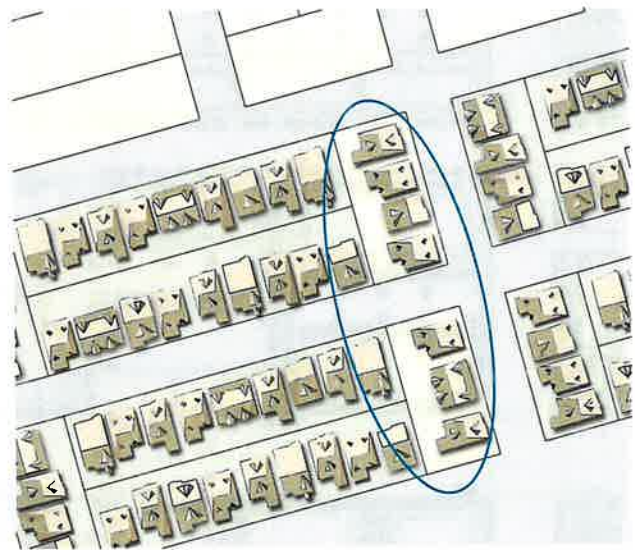
Adaptive reuse of a heritage home incorporated into the community.

3.2 Site Planning

Site planning plays an important role in how a development is experienced and how it functions, including elements such as building placement, site access, and landscaping.

3.2.1 Site Layout

1. Encourage community permeability by providing internal pedestrian connections through development sites which connect to external sidewalks, parks, and trails. These mid-block connections shall be a minimum of 4 metres wide, with a paved path of at least 2 metres.
2. The edges of a development, either residential or non-residential should reflect and complement the type of use at grade.
3. In order to minimize the visual impact of long blocks, the lots located on the end should be turned 90-degrees to face the flanking road.
4. Provide a safe, clear, and accessible site circulation system for pedestrians, cyclists, and vehicles, including connections to the surrounding street network, public sidewalks, transit stops, and parking areas. Pedestrian and bicycle movements should be prioritized through design and signage.



Turn lots on the end of the block 90 degrees to minimize the visual impact of long blocks.

5. Create a pedestrian-scaled environment by arranging buildings to create comfortable and protected pedestrian spaces that provide a sense of enclosure.
6. All pedestrian connections and entrances shall be universally accessible. If ramps are needed, they should be incorporated into the building design. Walkways should be distinguished from driveways through a change in material or by using a planted or sodded edge.
7. In mixed-use areas consider flexible spaces or opportunities to reclaim or re-purpose underutilized roadways and excess parking spaces. These roadways and parking spaces may be used to create additional public space for benches, planters, landscaping, bicycle parking, and café tables seating, where feasible.
8. Enhance wayfinding by using buildings as gateways and landmarks, public spaces as focal points and streetscapes to frame significant views.
9. The design of shared mailbox facilities should consider:
 - a. Locations where they act as an integral component of the streetscape, or in central areas such as an amenity area or park space. Seating and waste receptacles may be provided, where appropriate; and,
 - b. Including landscaping and/or privacy fencing as a buffer when located at a corner or end lot.

3.2.2 General Site Landscaping

Landscaping design should reinforce the structure of the site with a focus on creating a safe, comfortable, and animated pedestrian environment.

1. Develop a comprehensive strategy for planting, built features, fencing, walls, paving, lighting, signage, and site furnishings.



Example of a site layout illustrating building placement, access, and landscaping.



Amenity areas and green roofs in a high-rise development.



Low wall and plantings define the private and public realms.



Street tree planting to buffer the sidewalk from the street.

2. Base planting strategies on year-round interest, hardiness, drought, salt and disease tolerance, and bio-diversity.
3. Preserve, protect, and incorporate existing healthy and mature trees into the building and landscape designs.
4. Minimize the use of hard, paved areas to reduce surface run-off and heat island effect. Permeable paving should be used wherever possible.
5. High-quality, durable materials are to be used for all landscape features such as paving, fences, walls, planters, site furniture, and shade structures.
6. Residential units should include landscaping elements, such as steps, low walls, and plantings to delineate private front-yards and to provide a transition between private areas and the public realm.
7. Consider green roofs for mid- and high-rise buildings. This will assist with reducing heat island effects and improving air quality and noise insulation.
8. Incorporate a combination of soft landscaping, planters, and trees along non-residential frontages to delineate and differentiate private open spaces, entrances, and individual units at grade.
9. Appropriate planting conditions such as soil depth, volume, and growing mediums must be provided for successful landscapes.
10. Provide a photometric plan that coordinates site, building, and landscape lighting to ensure pedestrian safety and comfort.
11. The design of lighting shall avoid light spill over onto abutting properties, and most importantly, adjacent residential neighbourhoods.

3.2.3 Fences and Walls

1. The design of fences and walls should be coordinated with building design and site layout.
2. Corner lot privacy fencing for single detached, semi-detached, and townhouse dwelling units shall be provided in accordance with the following criteria:
 - a. Provide for screening of rear yard amenity areas on all corner lots where the rear yard is exposed to the street;
 - b. Ensure the fence meets the side of the house at the rear corner of the unit;
 - c. Ensure a height of 1.8 metres or as recommended in a noise attenuation report approved by the Town;
 - d. Fencing must be consistent throughout a development;
 - e. Design to include a gate on the portion of the fence that returns from the lot line to the side wall; and,
 - f. The exact location and design of corner lot privacy fencing will be approved by the Town and reflected in the Subdivision Agreement.
3. Avoid using noise attenuation fences or walls if possible. Noise attenuation fencing shall be provided if no other solution is possible and should integrate pedestrian connections to the adjacent communities, as appropriate.
4. Where noise attenuation fences or walls are used they should be integrated with the design with their surroundings. Coordination in the design, colour, texture, and plantings is encouraged.



Decorative fencing with a gate.



Side yard fencing.



Noise attenuation fencing .



Single detached dwelling with front porch and side driveway.



Example of an upgraded elevation with variety in roof lines, materials, and a front porch.



Example of a semi-detached dwelling.

3.3 Low-Rise Buildings

The following guidelines apply to low-rise buildings up to 3 storeys in height.

3.3.1 General Guidelines

1. All low-rise buildings shall demonstrate design excellence and compatibility with the surrounding context. Architectural detailing, landscape treatments, colour, and building materials shall be representative of the highest quality possible.
2. The height difference between adjacent low-rise buildings on the same block should not vary by more than 1 storey to maintain a consistent street wall.
3. Where side and rear elevations of units are exposed and visible from a public space or visible from a Regional Road, they shall have upgraded elevations that are consistent and continuous in design, quality, and material as the front elevation.
4. Locate garages and driveways on a Local Road or Rear Lane, off Arterial or Collector Roads.
5. Screen utility meters, air conditioning units, and similar features from public view and integrate into the design of dwelling units through the use of wall recesses, enclosures, screening, or inseting within the building walls. Rear lane units shall locate utility meters at the rear lot line.

3.3.2 Typologies

3.3.2.1 Single Detached, Semi Detached, and Duplex Dwellings

1. Design dwellings to frame the street edge with a consistent setback, and have front doors, windows, and entry features facing the road to create a consistent street wall.
2. Design the front elevation of the dwelling so that its front entrance and architectural elements reduce the visual dominance of the garage and driveway.

3. Garages shall not protrude beyond the main front wall of the dwelling unit. Garages shall be set behind or flush with the main building face or accessed from a rear lane.
4. The setback to the main building face should be 3.0 to 6.0 metres from the edge of the right-of-way. The main building face could be the main front wall, second floor room over or beside the garage, or a significant element such as a roofed porch or verandah.
5. Porches, stairs, canopies, and other entrance features may encroach into the required setbacks, a maximum of 1.5 metres.
6. Design driveways to reduce the amount of asphalt on front yards and enhance the visibility of the street.
7. Semi-detached and duplex dwellings with a front facing garage and driveway should pair the garages to maximize the extent of continuous green planting area.
8. Ensure semi-detached and duplex dwellings have a single unified roof form and continuous and consistent architectural details and materials for both dwelling units.
9. Duplex buildings shall have separate entrances for each unit.

3.3.2.2 Townhouses and Other Multiple Unit Dwellings

1. Coordinate the siting, massing, and facade design of townhouse units on a block-by-block basis.
2. Articulate the elevation of the townhouse block in a manner that provides variation between units with common characteristics that visually unites the block.
3. Utilize variety in the design of roofs through the use of traditional gables and dormers, or more contemporary designs that include cantilevers and parapet details to break up the massing of units within a block. The main roof should appear as one roof where possible and reflect the architectural style of the unit block.



Duplex building with two units.



Example of front drive townhouses.



Rear lane accessed townhouses.



Architectural detailing gives visual interest to the facade.



Corner unit with an upgraded elevation.



Live-work units with consistent architectural details on the facade.

4. Blocks of street townhouses should be limited to a maximum of 8 units, with 6 units preferred. The length of the townhouse blocks should not exceed 50 metres, unless it is essential to the architectural style of the townhouse block.
5. Orient blocks of attached townhouse units to the street with integrated front garages accessed from the street. For rear lane townhouses an attached or detached garage will be located at the rear of the block and accessed from a lane.
6. Front garages shall not exceed 50% of the width of the unit and should be paired to allow for more substantial front yard green space. Garages shall not protrude beyond the main front wall of the dwelling unit.
7. Orient the main front entry of interior units to the front lot line or higher order street. The entry of the end unit shall be oriented to the exterior lot line when on a corner lot. Where a dwelling unit flanks a private street or laneway, the main entrance shall face the front lot line.
8. Ensure rear lane accessed garages are complementary in design and building material with the principal dwelling.
9. Consider outdoor amenity areas in the form of second floor decks or rooftop patios for townhouses with an attached garage in the rear over traditional rear yard amenity areas.

3.3.2.3 Live-Work Units

Live-Work units introduce a flexible built form use that allows for the unit to be used fully as a residence or a residence above with retail, commercial or office uses at grade. Live-Work units are ideally suited for the mixed-use context in the Urban Centres and along the Mixed-Use Corridors.

1. Design Live-Work buildings to support pedestrian activity through minimal front yard setbacks, pedestrian weather protection such as canopies, and enlarged clear glazed windows and pedestrian-scaled detailing for commercial space.

2. Provide on-street parking by using lay-by parking with resident parking provided at the rear of the building and accessed from a lane or a private road.
3. Ensure Live-Work units have continuous and consistent architectural details and materials for the entirety of the block.
4. Screen mechanical equipment including air conditioning units and utility meters or locate away from public view.

3.3.2.4 Low-Rise Apartment Buildings

1. The design of the building and the site layout shall consider overall form, massing and proportions, and rhythm of major repetitive building elements to create a streetscape that supports the pedestrian scale.
2. The majority of the main building facade shall front the abutting street. The implementing Zoning By-law may establish a front yard build-within zone along the street line to ensure pedestrian comfort and streetscape activation.
3. Locate and orient primary building entrances to public roads and design to be visible and accessible to the public.
4. Locate a visitor drop off area at the side or rear of the building with lane access or private drive.
5. Locate parking below grade, where possible. Locate visitor parking, loading, and service areas in areas of low public visibility in side or rear yards and set back from the front facade of the building.
6. Where it is only possible to provide parking at grade, it shall be screened from street view through the use of landscaping, including features such as wrought-iron/metal fencing with masonry pillars and landscaping or a similar combination that is consistent with the building's architectural style.



Live-work units with lay-by parking.



Low-rise apartment with the main building facade abutting the street.



Low-rise building with balconies and entrances along the street.

3.3.3 Siting and Setbacks

1. Integrate existing topography and natural features into the development, and minimize alteration to the existing grading of the site, if feasible.
2. Locate dwelling units and townhouse blocks close to the street edge to create a pedestrian-oriented streetscape.
3. Orient dwelling units and townhouse blocks to face the public realm, and particularly any adjacent streetscape, pedestrian connection or open space, to promote a high level of comfort and create a safe environment.
4. Ensure the front yard setback of new units is consistent with that of adjacent units. If there are differing setbacks on adjacent lots, the new unit should be located to act as a transition between the differing setbacks.
5. Where infill development occurs through the severance of large lots into smaller lots, the resulting lots should reflect the rhythm and scale of lots in the surrounding area.
6. Where lot depths permit, dwellings on long, straight streets shall be designed to give the appearance of a diversity of setbacks, through architectural details and permitted encroachments.
7. The following elements may be permitted to encroach into front, rear, and exterior side yards, when appropriate:
 - Bay windows;
 - Balconies or decks; and,
 - Porches of up to 1 storey.
8. Where the first floor of the dwelling or townhouse unit is within 3 metres of a sidewalk, the entry of the unit shall be raised a minimum of 0.9 metres to a maximum of 1.2 metres above the sidewalk grade. The change of grade should be reinforced through landscaping features.
9. Maintain consistent side yard setbacks along the streetscape. If there are differing setbacks on adjacent lots, the side yard setbacks of new units should be the average distance of those on either side of the development.
10. Increase side yard setbacks at pedestrian links and public open spaces.
11. Low-rise developments are to have front-to-front or back-to-back dwelling configuration along streets, lanes, or around open spaces.
12. Avoid front-to-back façade configurations where possible. If necessary, the 'rear' facing units of front-to-back façade configurations are to include:
 - Recessed garages;
 - Enhanced landscaping; and,
 - Upgraded façades.
13. Locate built form to minimize the need for noise attenuation walls.
14. Upgrade the façade treatment for side and rear elevations visible from public areas.

3.3.4 Private Outdoor Amenity Space

The design of private outdoor amenity areas, such as balconies, terraces, back yards, or gardens provide an important extension to the livable space of a dwelling unit.

1. Design private outdoor amenity spaces to have direct access to sunlight and sky view.
2. Avoid a 'rear yard' condition along streets and parks/open spaces.
3. Locate private outdoor amenity spaces for family-sized units so that they have views and access to outdoor play areas, where possible.
4. Design roof top private amenity spaces to limit overlook into adjacent neighbourhoods.
5. Inset or partially inset balconies to offer greater privacy and shelter from wind, reduce the building bulk and minimize the impact of shadow on other amenity spaces below.



Garages should not dominate the facade.

3.3.5 Garages and Driveways

The design of garages can have a major impact on the visual character of the individual dwelling and the collective streetscape. A cohesive streetscape where garages compliment instead of dominate the streetscape is intended.

3.3.5.1 Front Garages

In order to minimize the presence of the garage, the following guidelines shall be applied for attached and detached garages accessed from the front yard.

1. Ensure garages are a natural extension of the design, massing, and materials of the main dwelling.
2. Ensure garages are set behind or flush with the main building face and do not project beyond the main wall of the dwelling. Garage doors facing a public road should be setback a distance of 6.0 metres from the right-of-way.
3. Design attached garages as follows:
 - a. De-emphasize their presence on the streetscape by recessing garages 0.5 to 1.5 metres from the main wall of the dwelling;
 - b. Accommodate a maximum of 2 garage doors for garages fronting the road, with a maximum width 50% of the dwelling width.



Front garages flush with the front facade.



Glazed top panels in the garage door.



Attached garage setback from the main building face.



Detached garage with consistent materials as the main dwelling.



Detached rear lane garage with a secondary suite.

Three garage doors may be permitted for single detached dwellings on a large lot but is not encouraged;

- c. Include two single garage doors separated by a masonry pier for double garages;
 - d. Setback a second storey built over the garage a maximum of 2.0 metres;
 - e. Consider glazed top panels or transom lights for all garage doors, especially for traditional style dwellings; and,
 - f. Utilize a consistent garage door throughout a townhouse block.
4. Detached garages are only permitted in the rear yard and interior side yard.
 5. Design detached garages as follows:
 - a. Provide access from either a rear lane or the street by a driveway;
 - b. Provide main cladding materials consistent with those of the exterior of the main dwelling;
 - c. Create staggering garage door depths and planes, and varying roof styles and details along lanes;
 - d. Ensure a minimum setback of 2.0 metres from the dwelling;
 - e. Ensure a minimum setback of 1.2 metres from the side lot line; and,
 - f. Ensure a maximum height of 2 storeys.

3.3.5.2 Lane-Accessed Garages

Garages that are accessed from a laneway can either be detached or attached to the main dwelling at the rear. Attached garages can be set into the house with access at the rear, or they can be attached to the main dwelling through a breezeway which forms a side courtyard for amenity space.

1. The minimum setback for garages accessed by a lane should be 0.6 metres from the lane right-of-way.
2. Side yard setbacks may be a minimum of 1.2 metres if the garage has doors or windows facing the side yard.
3. Side yard setbacks may be a minimum of 0.3 metres if the garage has no doors or windows facing the side yard. A nil setback is allowed where the garages on abutting lots are attached.
4. Where possible, pair garages to allow for increased rear yards or an outdoor parking pad.
5. The maximum number of attached garages on adjacent lots should be three.

3.3.5.3 Driveways

1. Ensure driveway widths are no larger than the interior width of the garage. A maximum driveway width of 3.0 metres shall be permitted for single car garages and a maximum driveway width of 6.0 metres shall be permitted for double car garages.
2. Utilize light-coloured paving material for driveways to reduce heat island effect.
3. Utilize porous or permeable pavement for surfacing driveways and parking areas instead of asphalt and concrete to reduce stormwater run-off.
4. Locate driveways as far as possible from parks, open space features, public walkways, schools, and intersections.



Rear garage attached to the main dwelling through a breezeway.



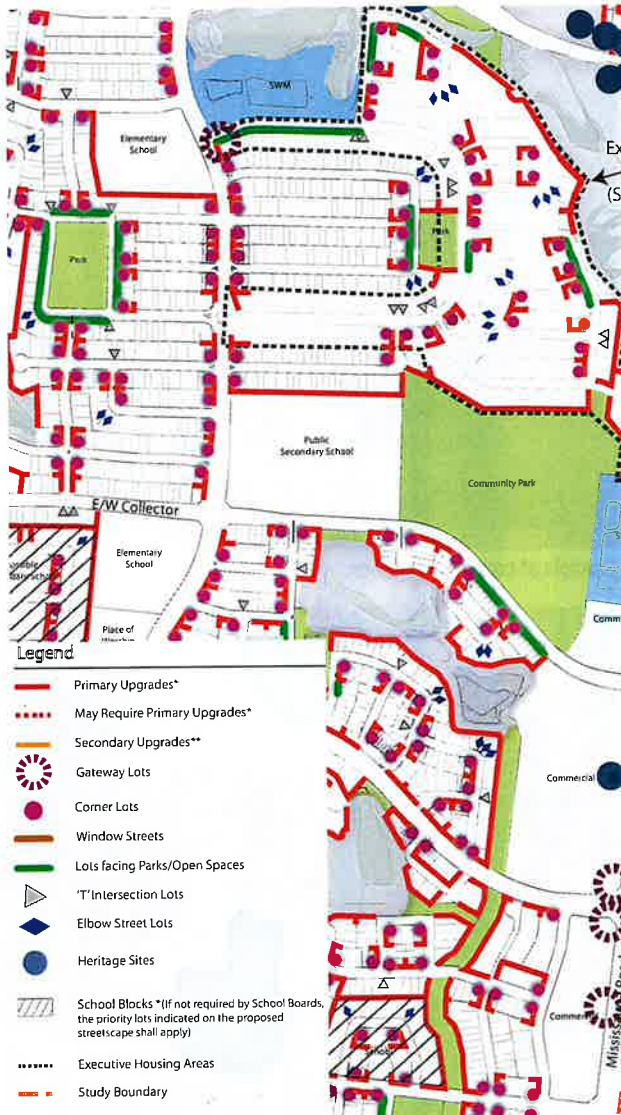
Lane based garages in groups of three.



Light coloured materials reduce heat island effect.



Priority corner lot designed to address both streets.



Example of a Priority Lot Plan that locates corner lots, gateway lots, lots and facing parks.

3.3.6 Priority Lots

Priority lots are those which are situated in prominent locations and are highly visible from the public realm. Priority lots include:

- Gateway lots;
- Corner lots;
- Lots which terminate at "T" intersections;
- Lots facing, adjacent to, or backing onto parks, open spaces, and pedestrian links; and,
- Window street lots.

1. Architectural and siting treatments for priority lots are recommended in order to promote a defined and attractive streetscape with visual focal points.
2. Developers shall submit final approved draft plans to the Control Architect for the preparation of a priority lot map and unit designs.
3. Where a townhouse is sited on a corner lot, the end unit flanking a street is defined as a priority lot.
4. In cases where a semi-detached dwelling is sited on a corner lot, both units should be defined as a priority lot.

3.3.6.1 Gateway lots

1. Dwellings on gateway lots shall be given special consideration in architectural design, massing, orientation, siting, and materials, and shall be of high architectural quality.
2. Utilize entry elements and porches to produce interest in the facade, as well as to help define the entrance to the neighbourhood.
3. Pair similar model units on lots directly opposite to each other to establish and enhance a gateway condition. Use stone or other quality materials as the main massing material for gateway units where possible.
4. Provide upgraded landscape features on gateway lots including decorative fencing, where appropriate.

5. Coordinate the materials of dwellings on gateway lots with those used on gateway features.

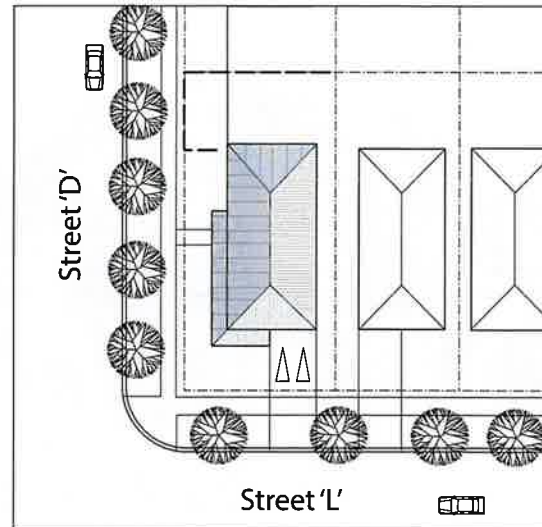
3.3.6.2 Corner lots

1. Wrap around windows, porches and other architectural treatments shall be considered for corner lot dwelling units.
2. Ensure active living spaces are designed for the rooms adjacent to the corner.
3. Locate main entry features on the flankage elevation where possible.
4. Coordinate privacy fencing design for all corner lots to prevent views into the private rear yard amenity area.
5. Articulate rooflines to include vertical features and elements such as a tower or turret, incorporate steeper roof slopes on traditional units, or break the roof plane in keeping with any wall projection for contemporary styles.

3.3.6.3 "T" Intersections

"T" intersections occur when one street terminates at a right angle to another.

1. Ensure the architecture on lots at the end of "T" intersections is of a highly articulated facade design such as coordinated fenestration, masonry detailing, and entry elements.
2. Incorporate special built form such as added height, turrets, or bay windows for "T" intersection lots.
3. Pair side yards to form a landscaped area at the terminus of the "T" intersection.
4. Locate garages away from the "T" intersection of the streets.
5. Provide larger front yard setbacks at the view terminus for "T" intersections.



Dwellings located at the intersection of two streets shall address both streets



Example of corner lot condition.



Dwelling unit at the end of a "T" intersection



A porch flanking a park creates "eyes on the park".



An example of front porches overlooking amenity areas.



Main entrance to the townhouse unit is located on the side elevation facing the street.

3.3.6.4 Lots Adjacent to Parks and Open Spaces

1. Ensure front, side, and rear elevations exposed to public spaces such as neighbourhood parks and village greens are highly articulated. Utilize a combination of fenestration, bay windows, material changes, and dormers in addition to other design elements to achieve the objective.
2. Ensure side and rear elevations adopt a similar design and use materials that are consistent with those used on front elevations. Architectural detailing such as corbelling should continue from front to side elevations, where visible to the public.
3. Ensure the location of porches, windows, and entry doors for units surrounding parks and village greens maximizes opportunities for overview.
4. Locate driveways of adjacent dwellings as far away as possible from the public space.

3.3.6.5 Window Street Lots

1. Ensure units facing or flanking a Regional or Collector Road are of a high architectural quality and given special consideration in design, massing, orientation, siting, and materials.
2. For units flanking a window street, the main front door should be visible from, and oriented to, the exterior side elevation of the dwelling with access to the sidewalk. Ensure the entries are articulated through the use of entry features such as projecting porches facing the street.
3. Ensure garages are recessed from the front wall of the dwelling to reduce their presence on the streetscape.

3.3.7 Building Design

3.3.7.1 Height and Massing

1. Ensure generally consistent height and massing along a street.
2. Provide appropriate transitions between all unit types to avoid drastic changes in height and/or massing.
3. Ensure appropriate design compatibility where different unit types are located adjacent to each other.
4. Limit the height of new dwellings in existing neighbourhoods to no more than one storey greater than the height of existing, adjacent buildings.

3.3.7.2 Articulation and Architectural Features

1. For infill development, ensure the elevation design reflects that of the adjacent homes. For example, the vertical rhythm and horizontal expression of the windows and door should be similar to surrounding homes.
2. For new subdivisions, the following requirements apply to single detached, semi-detached, and townhouse dwelling units:
 - a. Each model shall have a minimum of 3 distinct elevations;

- b. Identical building elevations shall be separated by a minimum of 3 lots;
 - c. Both units of a semi-detached dwelling shall share the same elevation;
 - d. Both units on a semi-detached dwelling are considered one elevation and shall be separated by a minimum of 3 lots;
 - e. Identical building elevations may comprise a maximum of 30% of a street block;
 - f. Exterior colour packages for single detached and semi-detached dwellings shall be separated by a minimum of 3 lots; and,
 - g. In a townhouse block identical exterior colour packages shall be separated by a minimum of 2 units to avoid repetition and monotony along the streetscape.
3. Ensure façade details throughout all building elevations are consistent with their intended architectural style.
 4. Avoid mixing architectural styles within a single dwelling.
 5. Any masonry details shall project a minimum of 12 millimetres from the wall face.
 6. Provide high quality and complementary light fixtures at main entrances and above garage doors.



Both units of a semi-detached dwelling share the same elevation.



A block of townhouse units with a variety of elevations and colours.



Front porch highlights the dwelling entrance and addresses the street.



Low wall, plantings, and articulated front porch along the street.



Variety of porch railing styles.

3.3.7.3 Porches and Entry Features

1. Articulate front elevations by highlighting front entries with features like porches, verandahs, arches, generous overhangs and massing elements such as a cantilevered or recessed upper storeys.
2. Front entry features with more than three steps shall be poured in concrete with masonry casing.
3. Ensure steps from a front porch are not located closer than 1 metre from a property line.
4. To ensure porches and verandahs are useable they should be a minimum of 1.5 metres in depth.
5. Encourage the use of a variety of column styles such as single columns, double columns, and columns with a masonry base.
6. Provide porch railings which are pre-finished and maintenance-free. Encourage a variety of railing styles and materials such as pre-finished aluminum, vinyl, wrought iron, painted or natural wood, or glass.
7. Provide prominently displayed municipal address signage that is visible and legible from the street.
8. Ensure that porch base materials extend to grade and are consistent with the chosen architectural style.

3.3.7.4 Windows and Doors

1. Use consistent window types, shapes, and styles on all individual dwellings and townhouse blocks.
2. Provide large ground floor windows where appropriate to the selected architectural style.
3. Provide windows which are maintenance free, thermally sealed, double glazed, and either casement, single or double hung.
4. Avoid the use of black glass.

5. Incorporate transom windows where floor heights permit.
6. Position windows on interior side elevations away from the windows of adjacent dwellings.
7. Ensure the window frame colour is compatible with the exterior colour package.
8. Include main entry doors and doorways that are scaled to complement the building's facade. Avoid sliding doors on front and exposed elevations.
9. Incorporate sidelights and/or transoms in single entry doors. Where these are not possible due to floor plan arrangement, provide a vision panel (glazing) in the entry door.

3.3.7.5 Roofs

1. Utilize a variety of roof forms, including cottage or hipped roof, front gabled, side gabled, cross gabled, mansard and flat roofs. The chosen roof style and pitch should reflect and complement the dwelling's architectural style.
2. Utilize a variety of roof features including accent gables, dormers, porches, and variation of roof ridges.
3. Coordinate the roof material and colour for detached garages with the main building.
4. For townhouse blocks, emphasize individual units through the articulation of roof lines (e.g. variations in roof slopes at end units, dormers, differing roof pitches, etc.) while maintaining a consistent roof style throughout the same block.
5. Provide frieze boards below the house and garage roof eaves on front and flanking elevations, as the architectural style permits.
6. Ensure all roof vents and plumbing stacks blend with the roof colour and are located on the rear slope of the roof, away from public view, where possible.



Articulated front entry elements include front steps, roofs, variation in colour and materials.



Corner windows, turrets, and gable roofs provide variety.

7. Use only flush mount skylights and ensure their colours are similar to the colours of the roof tiles.
8. When solar panels are visible to the public avoid aluminum frames and white backing sheets. Choose colors that are similar to those of the roof tiles and, when feasible, set photovoltaic panels flush to the roof.

3.3.7.6 Foundations

1. Exposed foundation walls are to be avoided.
2. The main wall cladding shall be a maximum of:
 - a. 10 inches (250 millimetres) from finished grade on elevations exposed to the public; or,
 - b. 12 inches (300 millimetres) from finished grade on interior or rear elevations not exposed to the public.
3. Foundation walls must be appropriately check-stepped along sloping grades.

3.3.7.7 Exterior Materials, Wall Cladding and Colours

1. Ensure materials reflect and complement the architectural style of the unit.
2. For traditional styles, provide a variety of high quality and complementary wall cladding materials such as brick, stone, stucco or cementitious siding.
3. For contemporary styles, include materials such as brick masonry with smooth finishes, high quality stone cut to larger calibre pieces, wood corrugated steel panelling, marble, metal, concrete, and metal roofing.
4. A maximum of two main wall cladding materials shall be permitted on a unit, with a third permitted for accents and architectural details such as gables, box-outs and bay windows.
5. Incorporate metal flashing which matches wall cladding or roof material.

6. Encourage a variety of colour palettes that include different, but complementary tones.

7. Ensure soffits, eave troughs, frieze boards, and fascias are the same colour throughout the dwelling.

8. Ensure material changes on exposed elevations occur at transition points, such as a change of plane.

9. Ensure rear and side walls exposed to public view are of a similar composition to the front wall.

3.3.7.8 Utility Meters and Mechanical Equipment

1. Where possible, locate utilities and meters in interior side yards, away from public view.
2. Locate utility and service meters discreetly by:
 - a. Integrating into the design of the building;
 - b. Screening through landscaping;
 - c. Recessing or enclosing in the porch entry or landing;
 - d. Installing below porch slabs and porch steps;
 - e. Grouping in one location in a wall recess, enclosure or, where appropriate, a small roof overhang; and,
 - f. Screening meters on exposed elevations by integrating them into a wall or below porches and steps, providing complementary landscaping, or placing them behind a change in plane towards the rear of the elevation.
3. Locate dryer vents, exhaust fans, furnaces and hot water tanks on rear elevations.
4. Locate air conditioning units in the rear yard, interior side yard, or on or under a rear deck.
5. For flat roofs locate air conditioning units on the roof, setback from the edge so they are out of sight from public view, where possible.

3.4 Mid-Rise and High-Rise Buildings

The following guidelines apply to mid- and high-rise built form. Mid- and High-rise buildings provide opportunities for framing and defining the public realm, while allowing for increased densities that more efficiently use land and infrastructure. They support mixed use communities, promote active transportation, and if done properly, generate livable pedestrian experiences.

Mid-rise buildings have a maximum height of 6 storeys and High-rise buildings have a maximum height of 12 storeys.

3.4.1 General Guidelines

1. Concentrate the greatest heights and massing along the frontage of a Regional, Collector, or Main Street.
2. Ensure the scale of mid- and high-rise buildings is compatible and sensitively integrated with surrounding residential uses in terms of building mass, height, setbacks, orientation, privacy, landscaping, shadow casting, accessibility, and visual impact.
3. To demonstrate mitigation of potential shadow or wind impacts on existing or proposed pedestrian routes, public spaces, and adjacent development technical studies may be required including a wind study and/or sun/shadow study.
4. Development transition requirements may be met using a combination of the following:
 - a. Separate mid- and high-rise buildings from low-rise buildings with a Local Road;
 - b. Locate less dense and lower scale buildings in locations adjacent to existing low-rise neighbourhoods;
 - c. Require a minimum 7.5 metre rear yard setback where mid- and high-rise development abuts low-rise properties;
 - d. Mitigate the actual and perceived massing impacts of a mid- and high-rise building by breaking up the mass



Example of stacked townhouses.



Example of a 6 storey mid-rise building.



Example of a 12 storey apartment building adjacent to low-rise.

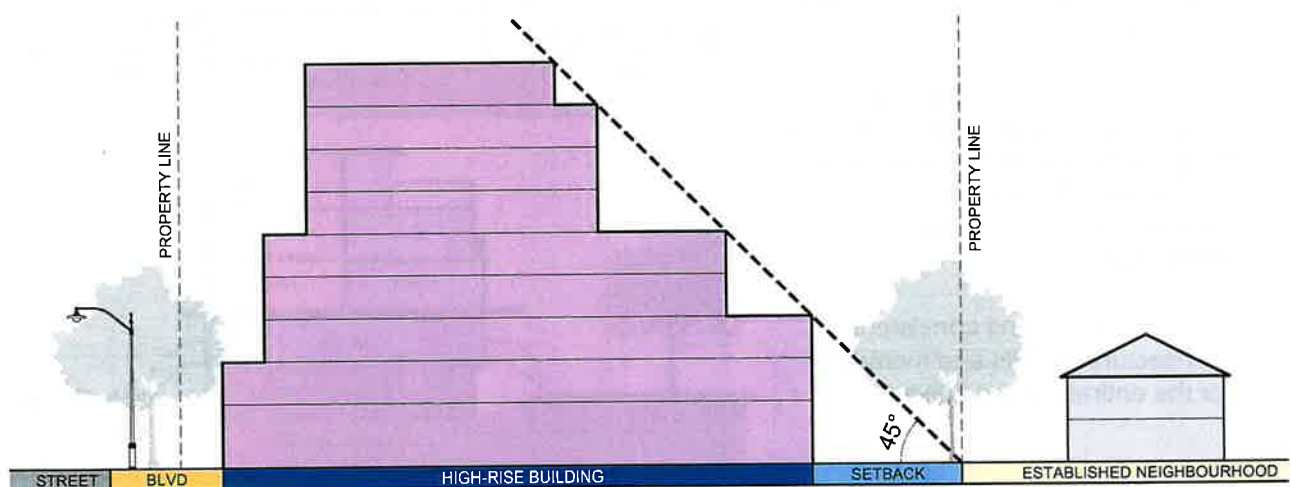
horizontally and vertically, through the creative incorporation of changes in materials, balcony and floor plate design, architectural features, and unit/amenity locations;

- e. Provide rear and side step-backs for upper storeys to provide contextually appropriate transitions from the mid- and high-rise buildings to the surrounding low-rise neighbourhoods; and,
- f. Provide high quality landscape treatment such as decorative fencing, trees, shrubs, grassed areas, and berming.



Use of step-backs to provide appropriate transition to adjacent uses.

- 5. Angular planes can be used as a tool to evaluate the massing and height transitions of proposed developments to ensure appropriate skyview, light, and separation. Development within the Urban Centres and Mixed-use Corridors shall apply a minimum 45 degree rear yard angular plane measured from the abutting property line where a building transitions to an adjacent low-rise residential area.
- 6. Ensure new development is compatible with adjacent and neighbouring development by siting and massing new buildings to avoid undue adverse impacts on adjacent properties particularly in regard to adequate privacy conditions for residential buildings and their outdoor amenity areas.
- 7. Use prominent built form to address gateway locations within the community. Within Urban Centres, 'paired' corner buildings on either side of a street to emphasize a sense of entry.
- 8. Locate and orient primary building entrances to public roads, and design to be visible and accessible to the public.
- 9. Screen rooftop mechanical equipment from view through architectural design that reflects the building's façade treatment. Add-on screening elements such as lattice are prohibited.



Angular plane diagram - 45 degree angular plane measured from property line.

3.4.2 Typologies

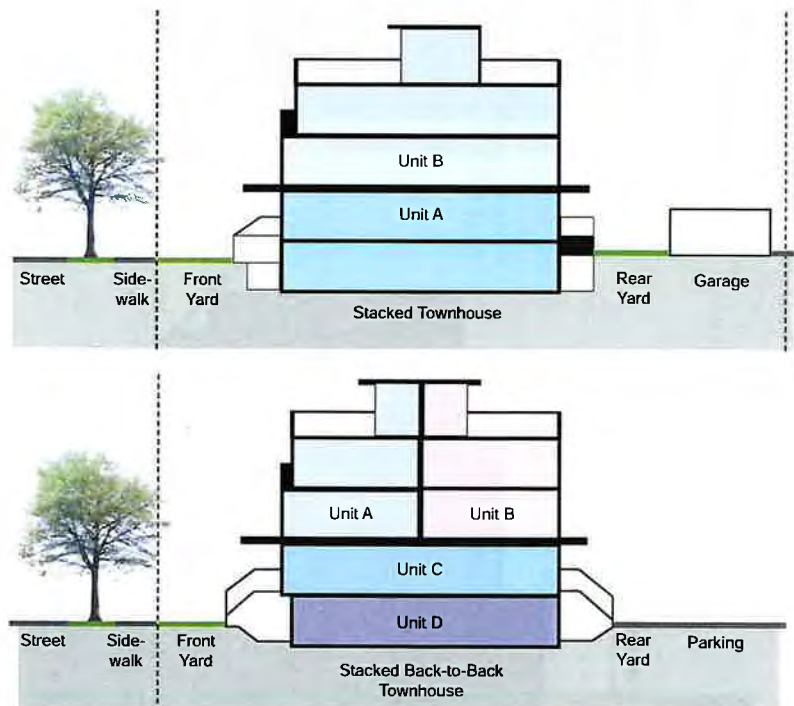
3.4.2.1 Stacked and Back-to-Back Stacked Townhouses

Stacked townhouses are typically a 3 to 4 storey building of attached units which are stacked one above the other and oriented to the street.

Stacked townhouses have units stacked vertically. This can include three units located on top of each other, a two storey unit stacked on top of a one storey unit, or a two storey unit stacked on top of two storey unit. Each unit has its own entrance at grade.

Back-to-Back Stacked townhouses share a rear and side wall and are two stacked townhouses placed back-to-back.

1. Articulate the elevation of the townhouse block in a manner that provides variation between units and reinforces common characteristics that visually unites the block.
2. Ensure variety in the design of roofs through the use of traditional gables and dormers, or more contemporary designs that include cantilevers and parapet details to break up the massing of units within a block. However, the main roof should appear as a single unified roof where possible and reflect the architectural style of the building.
3. Locate and orient windows, decks, and balconies to limit overlook into nearby windows and amenity spaces of adjacent properties while enabling "eyes on the street" for common public areas.
4. Limit stacked townhouse buildings to 3 to 8 units per block and the length of the townhouse block should not exceed 50 metres, unless it is essential to the architectural style of the building.
5. Use continuous and consistent architectural details and materials for the entirety of the building.
6. Locate attached garages at the rear of the building to be accessed from a lane or private drive.
7. Stacked back-to-back buildings shall have underground parking with direct access to each unit.
8. Limit the use of retaining walls, particularly along street frontages, parks, ravines and other areas of the public realm.
9. Where retaining walls cannot be avoided, provide them in the form of low terraces with the total height not to exceed 1 metre. Incorporate intensive soft landscaping in these low terraces and construct with durable and attractive materials.
10. Provide prominent, well-designed and integrated building entrances such as porches, porticos, or canopies along the building frontage.
11. On corner or double-fronting sites, locate building fronts and entrances facing both streets. Buildings on corner sites require additional attention to detail to enhance the corner.



Typical sections for Stacked and Back-to-Back Stacked townhouses.



Example of podium townhouses.



Examples of 4 and 6 storey residential buildings.



Example of a 6 storey residential building.

3.4.2.2 Podium and Liner Townhouses

These dwellings are typically 3 to 4 storey townhouse buildings that wrap around or are located at the base of an apartment building or parking structure.

1. Ensure podium and liner townhouses have continuous and consistent architectural details and materials among units, coordinated with those of the main mid- or high-rise building.
2. Utilize podium and liner townhouses as a residential veneer to create a 'street' or 'ground-related' façade to enhance the pedestrian realm of mid- and high-rise buildings.
3. Provide parking in a structure associated with the mid- or high-rise building.

3.4.2.3 Mid-Rise and High-Rise Buildings

These buildings are multi-storey structures that contribute to complete communities, provide a mix of housing and activity, and are built at densities that improve the viability of transit.

1. Mid- and high-rise buildings may include commercial and office uses at grade and multi-unit residential above or behind. Design ground floors to be appealing to pedestrians and include uses that are more active in terms of pedestrian traffic, such as commercial/ retail, personal service, and restaurant type uses on the ground floor.
2. Provide retail and service commercial uses on the ground floors of buildings to bring animation to the street and encourage pedestrian activity. Such uses should have a minimum 4.25 metre floor-to-ceiling height.
3. Ensure residential entrances are clearly distinguished from the commercial entrances through building design and locate at the front or side of the building.
4. Balconies on all storeys above grade are encouraged.
5. Locate visitor drop off areas at the side or rear of buildings with lane or private drive access.

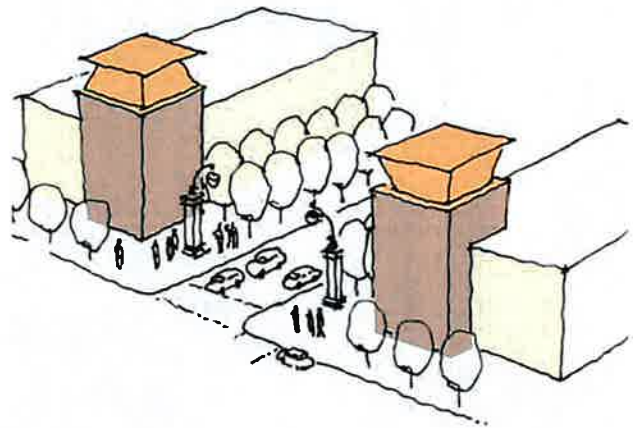
6. Design interior courtyards to maximize sun exposure through the massing and location of tall building elements.
7. Private outdoor amenity spaces can be provided in a variety of forms including front verandas on buildings where the podium is designed to incorporate townhouse units, roof-top decks, balconies, or a design with similar intent. See guidelines under 3.4.5.

3.4.3 Siting and Setbacks

1. Locate buildings close to the street edge to frame and animate the public realm. Where it supports a high quality public realm, new development should generally maintain and reinforce existing setbacks by aligning the base with adjacent building bases, or by placing it at the average distance between those of adjacent properties.
2. Organize and design the site (internal circulation and arrangement of structures) to reflect the urban context and fabric of the surrounding neighbourhood.
3. Provide mid-block pedestrian connections and multiple access-points to enhance visual and pedestrian permeability.
4. Include the provision of a minimum separation distance of 25 metres between high-rise buildings on the same site and a minimum tower setback of 12.5 metres from side and rear property lines. This will maximize sky views and natural sunlight, provide proper privacy, and minimize wind and shade impacts on surrounding areas.
5. Allow balconies to encroach on the 25 metre separation between buildings, while not contributing excessively to the building massing.
6. Use prominent built form to address gateways and other key locations within the community. On larger sites, create 'paired' corner buildings on either side of a street to emphasize a sense of entry or to distinguish one street district from another.



9 storey building located close to the street edge.



Paired corner buildings to emphasize a sense of entry.



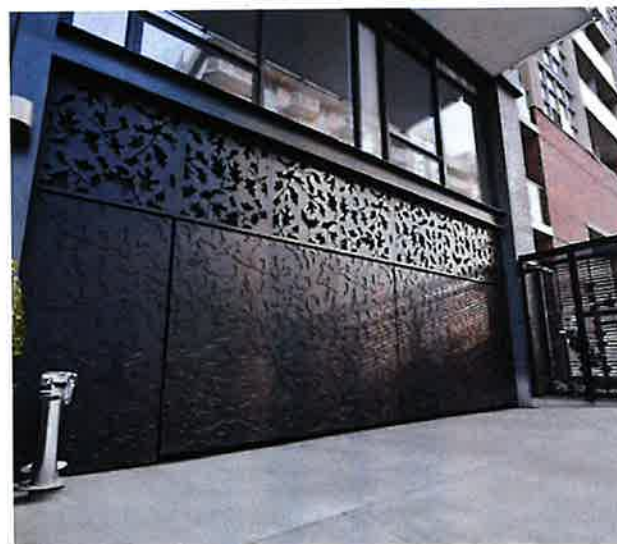
Corner building as a gateway feature.



Parking for stacked townhouses located in a rear lane.



Parking lot with smaller courts, plantings, and decorative paving.



Underground parking garage access screened by decorative door.

3.4.4 Access, Parking and Servicing

1. Provide access to parking, servicing and loading from the rear of the building, or a laneway where possible. On corner sites, provide access from secondary streets provided the entrance facilities are well integrated into the rest of the frontage.
2. Encourage lane-based and/or underground parking. Where not feasible, at grade and structured parking above grade may be permitted at the back of the building, preferably lined with active uses along all public frontages.
3. Locate and screen parking, loading, utilities, and servicing areas away from the public view through a combination of soft and hard landscaping, as well as other integrated architectural elements such as walls and pergolas.
4. Facilities for handling, storing, and separating waste and recycling should be integrated into the building design and screened from public view through landscaping and architectural elements.
5. Where it is only possible to provide parking at grade, split the surface parking into small courtyards by using walkways, public art, or landscaped strips.
6. Avoid vehicular site access from higher order roads. Provide access from local roads or rear lanes where possible.
7. Consolidate vehicular entrances to serve multiple buildings in order to minimize the number of interruptions to the street wall and sidewalk network. Limit the number of accesses from the same street to two.
8. Design underground/above ground parking ramps and service entrances as part of the building façade.
9. Provide long-term bicycle storage inside the building and short-term bicycle parking areas and racks close to entrances and external to the building.

3.4.5 Building Design

3.4.5.1 Height and Massing

Mid-rise and high-rise buildings are generally comprised of a podium, tower, and top.

1. The height of the podium, and the tower step-backs above, should generally reflect the established streetwall. Ensure the height of the podium matches existing adjacent structures to reinforce the pedestrian scale of the streetscape.
2. Where no established streetwall exists, the minimum height of the podium shall be 3 storeys to frame the streetscape.
3. For mid- and high-rise buildings with retail or other active uses at grade, provide a minimum ground floor height of 4.5 m. Residential ground floors should be a minimum of 4.0 m in height.
4. The tower should step-back a minimum of 3 metres from the podium to differentiate between the building podium and tower and to ensure usable outdoor amenity space.
5. Consider an additional step-back for buildings taller than 8 storeys in height.
6. Provide a height transition towards adjacent existing or planned built form. Refer to Guideline 3.4.1.5 for Angular Plane application.
7. The top of the building defines the tower while further distinguishing a unique and interesting skyline. Design the top of buildings to include a variety of elements, such as step-backs, material variations, lighting, and other architectural elements to reinforce a strong presence at the top of the building.
8. Where possible, include outdoor amenity space within the top of the building, including balconies, patios, terraces, and rooftop gardens.
9. All mechanical penthouses should be designed and clad with materials that complement the main building façades.

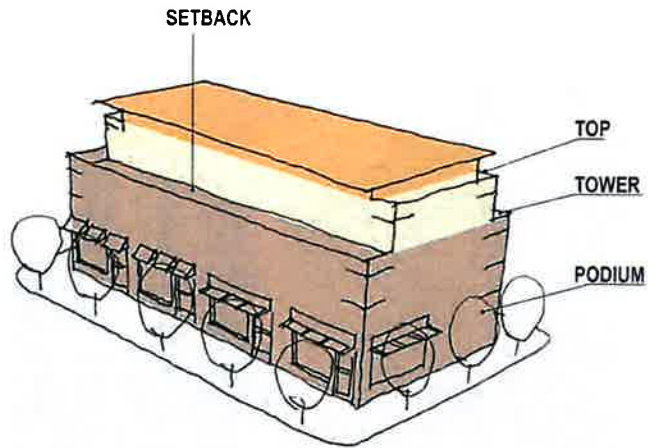


Diagram illustrating the building components of podium, tower and top.



The height of the podium on the mid-rise building should match the adjacent dwellings.



Top of the building designed with visual interest.

10. Locate mechanical rooms to the centre of the building rooftop and integrate into the rooftop design so they are not visible from the public realm.
11. Avoid blank or long expansive elevations which are exposed to the public view. Where unavoidable, consider art or special wall treatments (i.e., screens, living walls, metallic or wooden textures).
12. For developments with more than one building, provide a range of heights and establish a height hierarchy related to site conditions and context.

3.4.5.2 Articulation and Architectural Features

1. To animate the public realm and promote safe environments encourage active uses at grade based on the street character (i.e., retail, commercial uses, day-care facilities, townhouses, etc).
2. Mitigate the actual and perceived impacts of mid- and high-rise buildings by breaking up the mass both vertically and horizontally through the creative incorporation of changes in materials, balcony and floor plate design, architectural features, and amenity locations.
3. Incorporate windows and balconies on all elevations, especially if exposed to public view.
4. Locate entrances strategically so they are highly visible and well connected to the public realm.
5. Provide a high level of glazing at ground level, especially for those areas related to lobbies, common/amenity areas, and non-residential uses (i.e. commercial uses).
6. Encourage weather protective design at grade and at the podium level through canopies, arcades, and cantilevers. Canopies located on the ground floor should be at least 1.5 metres deep.

3.4.5.3 Exterior Materials

1. Ensure high quality and durable materials are used on all elements and elevations of the development.
2. Select materials to complement the architecture, character, size, and style of the building, as well as the streetscape.
3. Maintain consistent materials between elevations.
4. Incorporate changes in materials to visually break-up the building massing.
5. Use reflective, low intensity colours on rooftops to reduce heat island effect and HVAC loads. Refer to 4.1.7 and 4.1.8 for cool roofing material and solar reflectance guidelines.
6. Minimize danger to migratory birds by:
 - a. Avoiding untreated reflective glass or clear glass that reflects trees and the sky;
 - b. Ensuring glass has visual markers and is not reflective within the first 12 metres of building height; and,
 - c. Locating and managing lighting to reduce reflections that might confuse migratory birds.



Changes in exterior materials lessen the visual impact of the building.

3.4.5.4 Developments within Heritage Contexts

1. Locate and design buildings to respect and complement the scale, character, form and siting of on-site and surrounding cultural heritage resources.
2. Use existing heritage buildings to inform the site plan and podium layout and design.
3. Building bases should respect the scale of the surrounding historic fabric.
4. When an existing building is adapted or incorporated into the base of a high-rise building, the size and shape of the original window openings and entrances should be maintained.

3.4.6 Private Outdoor Amenity Space

Private outdoor amenity spaces should have access to sunlight, be comfortable, and designed to afford a level of privacy.

1. New multi-unit residential development shall provide shared space for both indoor and outdoor amenities.
2. Design private outdoor amenity spaces to:
 - a. Have direct access to sunlight and sky view;



Example of a building with inset balconies to minimize shadowing the balconies below.

- b. Mitigate impacts on the public realm and neighbours - increased facing distances between buildings may be required to reduce impacts;
 - c. Provide generous and well-designed landscaped areas to offer privacy, screening, and attractive interface with the public realm; and,
 - d. Include railing designs to help increase privacy, screen items from view, and reduce risk of bird strikes.
3. Raised terraces shall provide an entrance to only one unit and provide privacy with planting and architectural elements and translucent or solid railings.
 4. Raised terraces shall be raised a minimum of 0.6 metres and a maximum of 1.2 metres.
 5. Design roof top private amenity spaces to limit overlook into the adjacent neighbourhood:
 6. Design roof top terraces with parapets, solid or translucent railings.
 7. Inset balconies or partially inset to offer greater privacy and shelter from wind, reduce the building bulk, and minimize the impact of shadow on other amenity spaces below.
 8. Limit the size and avoid continuous projecting balconies, especially on residential streets, or when a private outdoor amenity space, pedestrian mews, and/or landscaped walkway is located below.



A consistent building setback frames the street..



The massing of buildings define the streetscape and create a sense of enclosure.



Corner buildings address both sides of the street with windows, signage, lighting, and a continuation of public walkways.

3.5 Commercial and Mixed Use Buildings

In Keswick, commercial and mixed use development is directed to the Urban Centres and Mixed-Use Corridors.

Urban Centres will generally contain a mixture of retail, service commercial, office, institutional/ community, and mid-rise built forms. The Mixed-Use Corridor along The Queensway will support the Urban Centres and generally accommodate higher intensity development with a mid-rise, mixed-use character, in a manner that is sensitive to the adjacent Existing Neighbourhood areas.

The Mixed-Use Corridor along Woodbine Avenue is intended to provide an array of retail shopping opportunities, community facilities, and mid-to high-rise development.

3.5.1 Building Design

3.5.1.1 Building Placement and Orientation

Building placement refers to the location of the building in relation to the street. The orientation and placement of buildings along the street can help to reinforce the public realm by enhancing the pedestrian environment and creating a sense of enclosure. Key guidelines for the orientation and placement of buildings are as follows:

1. Use mixed-use buildings and smaller scale retail and commercial stores to frame the street with a consistent building setback.
2. Ensure the siting and massing of buildings provides a consistent relationship, continuity, and enclosure to adjacent public roads.
3. At key corner sites, sidewalk cafes, kiosks, and street vendors are encouraged, and larger setbacks may be permitted. The area within the front yard setback should be hardscaped with paving for visual extension into the sidewalk.
4. Ensure buildings located adjacent to, or at the edge of parks or urban squares provide opportunities for overlook into the public space with windows and doors. The massing,

siting and scale of these buildings should create a degree of enclosure or definition appropriate to the type of open space they enclose.

5. Ensure primary entrances to buildings are clearly visible and located on a public road or onto a public open space for reasons of public safety and convenience. Secondary doors, such as those that face the parking area, emergency exits, and service doors should be designed to blend in with the building façade.
6. Ensure access to primary building entrances from sidewalks and public open space areas are illuminated, convenient, and direct with minimum changes in grade.
7. No parking, driveways, or lanes should be located between the buildings and the street. Exceptions may be granted for large buildings on large sites with multiple buildings where the larger buildings are be situated to the interior of the block with smaller buildings facing the street.
8. Locate patios along primary streets in areas that maximize sun exposure and effectively animate the public realm.
9. Provide accessible and secure bicycle racks and parking at retail, commercial, and employment developments, as well as at other key locations to promote active transportation.

3.5.1.2 Building Articulation, Massing, and Architecture

The aesthetic qualities of the building, its façade, roof line, windows, and access points are all vital factors in how the public perceive a building and how that building impacts their experience of the street.

1. Divide building frontages that exceed 12 metres in width into functionally and visually smaller units through the use of façade articulation, internal courtyards, and networks of connected walkways and landscaping.



Primary entrances located along the street frontage.



Building entrance is located on the corner of the building and wrap-around windows continue along both sides of the street.



Parking is accommodated through on-street parking with surface lots located in the interior of the site.

2. Articulate large walls visible from the street through various treatments such as offsets in massing, façade, and fenestration treatments.
3. For stand-alone commercial uses, minimize the building footprint by providing a multi-storey building in order to deliver compact form and conserve land.
4. Require a high level of architectural quality for the façade of buildings located at corner sites along Regional and Collector Roads.
5. Design sites with multiple buildings to reflect a consistent architectural theme. Similar building elements could include colours, materials, signage, and the base and top of buildings. Design individual buildings to offer visual interest and variety in design through architectural features.
6. Ensure consistent high quality building design and architectural elements on all building elevations, particularly on facades in public view or backing onto residential properties.
7. Establish a rhythm of minor breaks or articulation along the façade, distinguishing one unit (retail or residential) or building component from the next.
8. Incorporate architectural elements to enhance the pedestrian environment such as canopies, overhangs, awnings, projecting display windows, architectural arcades, and colonnades. These elements should be designed as integral parts of the building in terms of form, style, material, and colour.
9. Steps and ramps should be architecturally incorporated into the building entrance.
10. Fully screen and locate garbage, recycling, loading, and service areas away from public view. These facilities should be located in the rear or side yards away from residential uses, major roads, and open space areas. Where possible, integrate these functions within buildings.
11. Incorporate garbage and recycling storage bins that can be accessed for pick up into the principal building design. Food waste shall always be stored in climate controlled rooms.

3.5.1.3 Drive-Thrus

1. Drive-thru sites and buildings are permitted along Woodbine Avenue. Drive-through sites and buildings should be designed to:
 - a. Locate buildings close to or at the streetline to define and support the street edge and facilitate pedestrian activity and access;
 - b. Align new buildings with the front facades of existing buildings;
 - c. Ensure an appropriate transition in setback from existing and adjacent buildings along the street;
 - d. Locate the main entrance directly off the public sidewalk;
 - e. Ensure walls visible from the street are transparent with windows, doors, and other forms of transparent building materials to maximize views in and out of the building enhancing the relationship between interior and exterior to support and animate the public street and sidewalk;



The building is articulated with a clearly defined material selection and architectural quality.

- f. Provide vehicular access and stacking lanes along the side or the rear of the building away from adjacent residential uses, streetscapes, and open spaces. Do not locate stacking lanes or driveways between the building and the street;
- g. Provide parking adjacent to the secondary entrance to the facility so it is not necessary for pedestrians who arrive by car to cross driveways or stacking lanes to enter the building;
- h. Locate utilities and service components such as transformers, loading, and garbage pick up at the rear or flank of the building out of view from the street and other public areas; and,
- i. Provide sufficient signage where necessary to indicate direction of vehicular travel, stop signs, or no entrance areas.

3.5.1.4 Storefronts

1. Provide retail and service commercial uses on the ground floors of buildings to bring animation to the street and encourage pedestrian activity. The floor-to-ceiling height of ground floors for all new buildings shall be at least 4.25 metres.
2. Locate entrances to stores and the ground floor of live-work units at grade and design to be universally accessible, highly visible, and clearly articulated.
3. Provide spill-out space around the base of buildings for uses such as patios, street furniture, and special events.
4. Where retail uses are provided at-grade, ensure a significant amount of the building frontage on the ground floor and at building base levels is glass to allow views of the indoor uses and create visual interest for pedestrians. Clear glass is preferred to promote the highest level of visibility.
5. Awnings or canopies are encouraged above windows and doors.
6. Ensure storefronts on corner sites address both street frontages through entries and glazing.



Active uses such as restaurants and cafes extend their uses onto the sidewalks with outdoor seating, planters, and distinctive paving.



High level of transparency with glass windows create visual interest along the street.



Awnings, canopies, and signage provide shade and weather protection for pedestrians.

3.5.1.5 Signage

Signage plays an important role in the overall image of any area. Signs contribute to the quality of individual buildings and the overall streetscape, and reflect the unique characteristic of their context. Signage should be subject to the following guidelines:

1. Integrate signage in the building design and ensure it complements the building's elevation, animates the ground level and enhances the streetscape.
2. Design signage to be consistent with respect to materials, size, location (on a building), lettering and lighting, while also allowing some flexibility for tenant branding.
3. Ensure signage lighting design complements the design of the building.
4. Direct signage lighting to limit light trespass to surrounding properties and to prevent light pollution.
5. Signage should add diversity and interest to the street and not overwhelm either the storefront or streetscape. Design building

signage to be compatible and complement the architecture of the building in its scale, material, consistency and design.

6. Signage should not obscure windows, cornices or other architectural elements.
7. Back-lit illuminated sign boxes are discouraged.
8. Projecting or hanging signs should be permitted to encroach over the streetline provided that they do not project more than 1.0 metre from the building. There should be a minimum 2.4 metre clearance between the bottom of the sign and grade.

3.5.2 Parking

1. Provide a variety of parking options, including on-street parking, underground parking, structured, and screened at-rear parking courtyards. Avoid the use of large surface parking areas, where possible.
2. Locate parking areas away from the street frontage, at the rear or sides of the principal building.



Lighting above signage is directed at the sign and complements the design of the building.



High quality signage is in keeping with the scale and material of the rest of the building.



Hanging signs encroach over the streetline and extend into the pedestrian realm.

3. Design surface parking to minimize environmental impacts by reducing parking area size, considering shared parking facilities with adjacent buildings, and providing preferential parking for fuel efficient vehicles.
4. Break large parking areas into smaller courts by providing walkways at a minimum interval of 8 rows of parking. Locate walkways flanking a lane or between 2 parking rows.
5. Use landscaping to break up parking areas to assist with reducing the heat island effect. Landscaping islands should have a minimum width of 2.5 metres.
6. Screen surface parking lots from roads, open spaces, and adjacent residential areas with the use of buildings, low fencing, architectural features, landscaping, berms, or other mitigating design measures, such as lowered parking surfaces with landscaped buffers.
7. Incorporate pedestrian walkways and landscaping into surface parking areas along primary vehicular routes to enable safe, barrier free, and direct movement to principal building entrances and the sidewalk.
8. Design walkways with a minimum width of 1.8 metres.
9. Where walkways cross drive aisles, they should be differentiated from the driving surface through the use of surface materials and colour.
10. Consider above or below grade parking structures where possible and feasible in efforts to conserve land, promote compact development, and reduce heat island effect.
11. Design and landscape parking facilities at-grade or in structures to complement the urban streetscape.
12. Incorporate active uses at-grade for above grade parking structures facing onto any Regional, or Collector Road, where possible.



Lowered parking surfaces and landscaped buffers help screen parking areas from street view.



Landscaped islands, pedestrian walkways with distinct paving, lighting, and plantings provide safe crossing through the parking lot.



Example of a parking structure set back from the street.

13. Where above grade parking structures abut a road, minimize the visual impact of the building through screening or by treating the building face like an occupied building through expressing an architectural vocabulary and material compatible with adjacent façades.

3.5.3 Servicing, Storage and Loading

Servicing, utility, storage, and loading are necessary components of all building sites. These areas need to be functional and easily accessible and their visual impact minimized through location and screening.

1. Coordinate, consolidate, and integrate loading docks, service areas, and storage within the building envelope, where possible.
2. Locate loading, service, storage, and utility areas away from public streets and screened from public view.
3. Ensure that waste collection vehicles have ample room to maneuver at the site planning stage to ensure that these functions do not spill over into either the public right-of-way or public spaces.
4. Provide access to servicing and loading areas from secondary streets or rear laneways. Include design treatments to minimize impact and improve safety for pedestrians and cyclists crossing these areas.
5. Locate all utilities underground. Where components of utilities must be located above ground, utility providers are encouraged to consider innovative methods of containing utility services on or within streetscape features.
6. For all restaurant uses, cooking ventilation systems, incorporate ecologizer, water wash, ultraviolet, or other equivalent odour extraction mechanisms that are sufficient to ensure that the resulting exhaust is substantially odour free and will not affect surrounding residents.
7. Integrate facilities for handling, storing, and separating waste and recycling into the building design.

8. Ensure waste facilities within an external structure are consistent in design, colour, and materials to the main building and are not in a prominent location.

3.6 Public Service Facilities

Public service facilities provide important opportunities for place-making and landmarking. With distinct architecture and high quality public spaces, and the potential to co-locate and share facilities, these sites can become hubs of activity and focal points for social interaction, gathering, and civic events.

Buildings serving these uses include schools, recreation centres, libraries, places of worship, and fire stations. Careful attention must be paid to the design of these structures to ensure they reflect the built quality and integrate with the scale of the surrounding neighbourhood.

3.6.1 General Guidelines

1. Site public service facilities prominently and where possible, to terminate views. Ensure buildings are sited to specifically differ from the surrounding urban fabric in order to emphasize their importance as landmarks.
2. Locate public service facilities in community hubs to promote cost-effectiveness and facilitate service integration and access to transit.



The public service facility building is located close to the road to frame the street edge.

3. Locate public service facilities close to the road to reinforce the street wall and define intersections.
4. Ensure public service facilities have direct access from the surrounding community through a comprehensive and connected active transportation network with linked trails and parks.
5. The site should be well landscaped and visible at the pedestrian level.
6. Locate vehicular parking at the side or rear of the building. Parking for cyclists should be located near building entrances and where visual surveillance can be maximized.
7. Provide drop-off areas for buses and cars in the public right-of-way where possible, but when located on site they should be at the side, and not the front of the building.
8. Ensure public service facilities contribute to the creation of compact neighbourhoods through multi-storey buildings in order to maximize the site and services, minimize building footprint, as well as contribute to an urban street condition
9. Ensure the site and building incorporates accessibly for all individuals of varying ages and abilities.

3.6.2 Building Design

1. Design public service facilities as special landmark buildings with high quality architectural design, materials, and finishes.
2. Locate the most active portions of the buildings facing higher order streets. Locate large portions of buildings such as gymnasiums or auditoriums to the sides, rear, or interior of buildings.
3. Incorporate architectural elements such as massing and special features to terminate important views and vistas.
4. Respond to the local context and site conditions when siting buildings. Where applicable, design buildings to respond to the site's topography.



Bicycle parking is provided in close proximity to the front entrance of the school.



Multi-storey building is massed around a central open space.



Example of the use of architectural features to denote landmark community facilities.

5. Ensure highly articulated façade design for all elevations exposed to public view. This includes changes in plane and materials, fenestration, projections, relief, and horizontal and vertical elements. Blank, uninterrupted walls shall be avoided.
6. For buildings located at corners, design elevations to equally address the two street frontages. Additionally, use prominent massing, height, architectural elements and detailing to emphasize these locations.
7. Provide integrated weather protection elements at main entrances and ensure they complement the building's design.
8. Ensure the front door of all public service buildings are connected by a walkway to the sidewalk and have direct access to transit stops.
9. Coordinate building materials and ensure they reflect, complement, and enhance the building's architectural style and detailing.
10. Provide a high level of visual transparency and permeability at eye level for lobbies by using windows and prominent entrances.
11. Utilize daylighting strategies, such as building orientation, uniform windows across the facade, or skylights to capture natural light and reduce the need for electric lights during the day.
12. Consider roof forms other than flat roofs to respond to the context and character of the neighbourhood, particularly where there is a heritage context, and to highlight the nature of the public or institutional building.
13. Where flat roofs are used, incorporate green roofs in the design of the building to minimize surface runoff, reduce heat island effect, provide noise insulation, and improve local air quality.
14. Screen rooftop mechanical equipment with materials that are complementary to the building or through parapet height where applicable.
15. Ensure the design of ancillary buildings and structures is coordinated with that of the principal building in terms of height, massing, architectural details, lighting, signage, materials, and colours.
16. Integrate signage within the building design and ensure it complements the building's elevation, animates the ground level, and enhances the streetscape.
17. Direct signage lighting to limit light trespass to surrounding properties and downcast to prevent light pollution.
18. Ensure signage provides a high level of clarity, visibility, and visual interest, and should aid pedestrians and drivers in navigating the area, especially at night.

3.6.3 School Sites

1. Minimize the land area required for school sites in order to promote compact development and conserve land. School Boards are encouraged to build more compact facilities including three storey elementary schools and buildings located close to the street.
2. Where possible, locate elementary school sites adjacent to a neighbourhood park so that playfields can be shared to promote compact development and minimize land area requirements. Explore the use of appropriate



Example of a multi-storey elementary school.

and innovative engineered turf material to increase the durability of the playfields and minimize maintenance requirements.

3. Consider shared parking lots for elementary school sites with neighbourhood parks, and secondary school sites with community parks, in order to reduce the number of parking requirements. Locate and site the shared parking lot to facilitate easy and safe access, and to minimize the need for crossing required by students.
4. Schools sites that are located adjacent to the natural heritage system should maximize the opportunity for using the natural heritage system for passive open space uses such as trails and trail heads.
5. Design schools to ensure safe pedestrian crossing and cycling practices. Whenever possible, ensure students are able to easily reach building entrances without crossing bus zones, parking entrances, and student drop-off areas.
6. Design school sites to provide for visitor parking and bus pickup and drop off in demarcated bays in the adjacent Collector Road right of way.

7. Locate schools near transit stops and mixed-use areas, where appropriate. Locate Secondary schools within a 200 metre walking distance of a transit stop.

3.6.4 Fire Stations

1. Locate fire stations in a prominent and visible location with convenient access to a Regional or Collector Road.
2. Ensure fire stations have a close relationship to the intended service area by integrating with the surrounding development, through appropriate architectural design, landscaping and buffering from residential buildings.

3.6.5 Places of Worship

1. Locate Places of Worship on Regional or Collector Roads along public transit routes in order to maximize transit ridership.
2. Consider the joint use of parking areas with adjacent uses in order to reduce land requirements and promote compact development, especially in mixed use areas.
3. Ensure the massing and scale of the building is compatible with the character of adjacent development, especially within low-rise areas through the use of similar setbacks, material selection, and the use of architectural elements.



Parking is located to the rear of the building off the main road and the front entrance to the building is directly connected to the public sidewalk.



4. GREEN INFRASTRUCTURE AND BUILDING

While sustainability is an overarching objective throughout the Guideline, this section provides guidance on green infrastructure and building practices and helps achieve the broad sustainability principles of the Official Plan and the specific policies as set out in the Secondary Plan.

As part of the strategy to achieve a high level of sustainability in regards to the reduction of energy, water, and waste within Keswick, the Green Infrastructure and Building Guidelines apply to both the private and public realm.

4.1 Energy Conservation

Provide for the reduction of energy use and consider the inclusion of alternative energy sources.

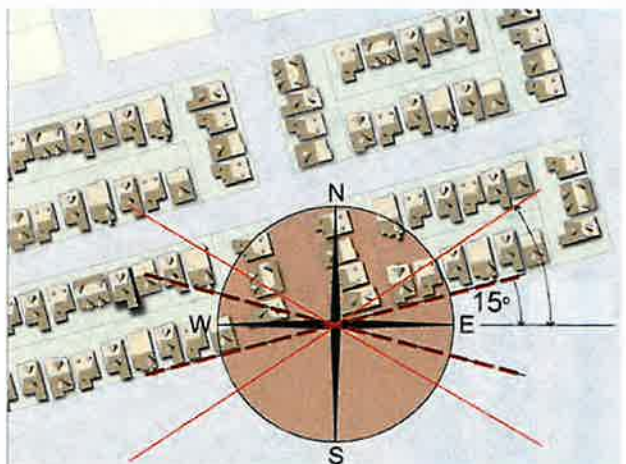
1. Where feasible, provide alternative community energy systems such as district energy, geo-exchange, sewer heat recovery, and/or inter-seasonal thermal energy.
2. Consider reducing demand for energy from the grid and encourage renewable energy production. Renewable energy sources that could be employed may include the use of solar thermal and photo voltaic equipment or wind power. Proposed alternative energy sources could be used in combination with energy from the grid.
3. Encourage passive solar building orientation to permit enhanced energy efficiencies by creating optimum conditions for the use of passive and active solar strategies. The integration of passive building systems is enhanced with buildings oriented to maximize the potential for sunlight and natural ventilation.
4. Where feasible, implement street and block alignment within 15 degrees of geographic east-west to maximize passive solar orientation of buildings front and rear windows.



Wind turbines on the roof of an office building.



Solar canopies in surface parking lots.



To maximize passive solar orientation the street and block alignment should be designed within 15 degrees of geographic east-west.



Solar panels on the roof of low-rise development.



Light coloured pavers assist with the reduction of heat island effect.



Charging stations for electric vehicles in mixed use areas.

5. Consider constructing all low and mid-rise residential buildings to be Solar Ready. Being Solar Ready means built with all the necessary piping and equipment that would be needed to install a rooftop solar power system.
6. Where available, consider using energy from renewable resources.
7. Reduce heat absorption through the use of cool roofs that are designed to reflect more sunlight and absorb less heat than a standard roof. Cool roofs can be made of a highly reflective type of paint, a sheet covering, or highly reflective tiles or shingles. Consider cool roofing material with a minimum initial solar reflectance of 0.65 and minimum thermal emittance of 0.90.
8. For a low sloped roof, typical of commercial and institutional buildings, the cool roof Solar Reflectance Index (SRI) value should be 0.64 and for steep sloped roofs, typical of residential, the SRI value should be 15.
9. Mitigate heat island impacts through the installation of light-coloured paving materials including white concrete, grey concrete, open pavers, and any material with a solar reflectance index of at least 29.
10. Implement the strategic use of deciduous trees or preserve existing trees to help with evapotranspiration and the shading of sidewalks and hard surface areas in the summer and solar access in the winter.
11. Charging stations that would supply electricity for electric vehicles are encouraged in new development. Provide charging stations in parking areas of mixed-use, office, institutional, or employment uses, or within underground garages for multi-unit residential buildings.
12. Consider paving driveways for grade related residential dwellings with light-coloured material to reduce heat island effect.

4.2 Water Use and Management

Compact development reduces impervious surfaces and makes it easier to protect natural areas which are important steps a community can take to maintain local water quality.

1. In order to promote water conservation, all new developments are encouraged to:
 - a. Achieve 10% greater water efficiency than the Ontario Building Code and to encourage through appropriate incentive programs, 20% greater water efficiency than the Ontario Building Code;
 - b. Restrict the use of potable water for outdoor watering;
 - c. Consider the use of water efficient and drought resistant plant materials in parks, along streetscapes, and in public and private landscaping;
 - d. Avoid use of turf grass areas, and when required, install drought resistant sod;
 - e. Increase topsoil depths and provide soil scarification;
 - f. Utilize native species; and,
 - g. Reduce the impact caused by new development on the natural hydrological cycle by installing permeable driveway and parking lot surfaces.
2. Encourage the implementation of Low Impact Development standards that emphasize the use of bioswales, innovative stormwater practices, constructed wetlands, at-source infiltration, greywater re-use systems, and alternative filtration systems such as treatment trains.
3. Implement a comprehensive rainwater and water recharge strategy in conjunction with required stormwater management facilities.
4. Consider strategies for stormwater retention and run-off such as:
 - a. Retain stormwater on-site through rainwater harvesting, on-site infiltration, and evapotranspiration;



Bioswales in the public right-of-way to improve infiltration.



Example of an innovative stormwater management facility.



Bioretention planters assist with street greening and have stormwater management benefit .



Landscaped island with a bioswale to filter run-off from the parking lot.



Example of the use of permeable paving material on a road.



Collect, store, and distribute rainwater in underground storage tanks.

- b. Direct flow to landscaped areas and minimize the use of hard surfaces in order to reduce the volume of run-off into the storm drainage system;
 - c. Store snow piles away from drainage courses, storm drain inlets, and planted areas; and,
 - d. Use infiltration trenches, dry swales and naturalized bioswales adjacent to parking areas to improve on-site infiltration.
5. Stormwater management quality control devices which require frequent operation or maintenance such as Oil Grit Separators are discouraged within the public right-of-way.
6. Introduce green infrastructure, such as bioswales, within the public right-of-way to enhance ground water infiltration and improve water quality as part of a comprehensive water management plan.
7. As a stormwater run-off management strategy consider the use of porous or permeable pavement instead of standard asphalt and concrete for surfacing sidewalks, driveways, parking areas, and road surfaces.
8. Consider the inclusion of third pipe greywater systems and rain water harvesting for watering lawns, and gardening, to reduce demand on potable water use;
9. Implement a rainwater harvesting program to provide the passive irrigation of public and private greenspace, including absorbent landscaping, cisterns, rain barrels, underground storage tanks, infiltration trenches, etc.
10. Consider the installation of subsurface basins below parking lots to enable stormwater to be stored and absorbed slowly into surrounding soils.
11. Where feasible, implement curb cuts along sidewalks and driveways to allow water to flow onto planted zones or infiltration basins.

12. Implement xeriscaping using native, drought-tolerant plants as a cost-effective landscape method to conserve water and other resources on a residential and community-wide level.

4.3 Lighting

1. Promote Dark Sky/Nighttime Friendly compliant practices to minimize light pollution and the intrusion of unwanted lighting on natural areas.
2. Consider high efficiency street lighting to reduce energy use.
3. Ensure lighting products (bulbs, fixtures) are compatible with existing lighting products used throughout the Town.
4. Consider opportunities for renewable energy use to reduce electric energy supply in the public realm, such as solar powered lighting for natural trails and park pathways.

4.4 Green Buildings and Green Sites

Promote innovative programs to encourage the design and construction of energy efficient green buildings and sites.

1. Consider Leadership in Energy and Environmental Design (LEED)® Certified or accredited third-party certification programs, such as LEED for New Development, LEED H, LEED NC, LEED for Schools Energy Star, and BREAM, etc.,..
2. Consider innovative residential development designs which contribute to affordability and energy and natural resource conservation.
3. Encourage synergies between buildings and site management practices that conserve water, reduce waste, and are energy efficient.
4. Green roofs are encouraged for high-rise residential, office buildings, as well as, public institutional buildings to minimize surface runoff, reduce heat island effect, provide noise insulation, and improve local air quality.



LED street lighting.



Solar powered lighting for trails and parks.



Green roofs reduce surface run-off on mid-and high-rise buildings.

5. Provide green roofs for 80% of all high density development. In high-rise residential buildings, design roofs as amenity areas.
6. Develop a heat island reduction strategy for community and public buildings to install green roofs with 50% coverage with the remainder of the roof covered with light coloured material. Light coloured roofs have a high solar reflectance which reduces energy costs and reduces heat island effect. Refer to 4.1.7 and 4.1.8.
7. In order to promote Energy Efficiency:
 - a. Where feasible, provide alternative community energy systems such as geoexchange, sewer heat recovery, or inter-seasonal thermal energy; and,
 - b. Development plans and building designs shall provide opportunities for south facing windows and building orientation to maximize potential for passive and active solar energy.
8. In order to promote Water Efficiency:
 - a. Strive to achieve a minimum 10% water savings beyond the Ontario Building Code requirements for new buildings;
 - b. Use Low Impact Development strategies to deal with on-site run-off and heat island effects;
 - d. Incorporate water efficient and drought resistant site landscaping by using native planting materials; and,
 - e. Pre-design for grey water pipe infrastructure.
- 9 In order to promote Green Materials:
 - a. Incorporate waste reduction work plans and construction best practices that reduce construction waste;
 - b. Incorporate green building material standards to reduce impact on the environment and ensure materials are purchased/obtained from a responsible ethical sources;

- c. Utilize low or non-volatile organic compound products in building construction and finishing to enhance indoor air quality; and,
- d. Source materials from certified local businesses.

4.5 Stewardship and Education

For new development in Keswick the following should be considered to support homeowner education and stewardship.

1. Create a Homebuyer's Environmental Instruction Guide that explains the unique environmental aspects of the development and special maintenance considerations.
2. Include an owner/tenant education package at the time of purchase or rental regarding activities to improve energy and water efficiency, access to transit, location of recycling station, etc. Coordinate with existing municipal and regional information
3. Include environmental builder specifications in all subcontracts.
4. Produce detailed sales and promotion materials that feature conservation aspects of the development.
5. Develop subdivision covenants that establish ground rules for the maintenance of shared open lands and individual lots.

4.6 Retrofitting Existing Private Properties

Flooding is an issue of concern for the residents of Keswick. The following guidelines provide suggestions for improvements to individual properties to protect from flooding. Retrofits to private properties must comply with site alteration by-laws and municipal stormwater management plans. All required permits must be obtained, where necessary.

Related Information can be found on the [Lake Simcoe Region Conservation Authority](#) website ¹.

1. Ensure the property is properly graded and that water drains away from basement walls.
2. Install a rain barrel to capture water runoff.
3. Ensure that downspouts extend a minimum 2.0 metres from the basement wall.
4. Install porous or permeable pavement or pavers instead of standard asphalt and concrete for surfacing walkways, driveways, or parking areas to assist with the capture and absorption of rain water.
5. Plant a rain garden to capture and store rain where it falls, filtering through the soil. Rain gardens should be located:
 - a. at least 3.0 metres from the dwelling unit;
 - b. at least 4.0 metres from a septic system; and,
 - c. 15.0 metres from any steep slope (more than 15%) on or around the property.
6. Plant rain gardens with native plants that are hardy to the area. The following is a list of native plants that are suitable for rain gardens. Related Information can be found on the [Toronto Region Conservation Authority](#) website ².

Grasses

- Big bluestem (*Andropogon gerardii*)
- Canada wild rye (*Elymus Canadensis*)
- Tufted hairgrass (*Deschampsia cespitosa*)
- Little bluestem (*Schizachyrium scoparium*)

Flowering herbaceous plants

- Wild columbine (*Aquilegia canadensis*)
- Butterfly weed (*Asclepias tuberosa*)
- White turtlehead (*Chelone glabra*)
- Showy tick-trefoil (*Desmodium canadense*)
- Spotted Joe-pye weed (*Eupatorium maculatum*)
- Oxeye sunflower (*Heliopsis helianthoides*) – sun
- Wild bergamot (*Monarda fistulosa*)
- Black-eyed Susan (*Rudbeckia hirta*)
- New England aster (*Symphyotrichum novae-angliae*)
- Swamp milkweed (*Asclepias incarnata*)

Shrubs

- Red-osier dogwood (*Cornus sericea*)
- Eastern ninebark (*Physocarpus opulifolius*)
- Nannyberry (*Viburnum lentago*)
- Common elderberry (*Sambucus canadensis*)

¹ Lake Simcoe Region Conservation Authority: <https://www.lsrca.on.ca/permits/stormwater-management>

² Toronto Region Conservation Authority: <https://trca.ca/news/complete-guide-building-maintaining-rain-garden/>

Appendix II

The mapping included in this Appendix was prepared as part of the background work for the Keswick Secondary Plan by Plan B Natural Heritage in 2018. The work carried out by Plan B was primarily a desk-top exercise, utilizing the existing information and data from the Town of Georgina, the Region of York, the Conservation Authority and the Province of Ontario. The mapping in this Appendix formed the basis of the Natural Heritage System identified on the Schedules of the Keswick Secondary Plan.

Since the production of the Background Mapping the delineation of the Natural Heritage System that is now articulated on the Schedules to the Keswick Secondary Plan has been refined through a review of detailed Environmental Impact Studies that have been accepted by the Town, approved Zoning By-laws and approved Draft Plans of Subdivision. As such, the Natural Heritage System originally identified in this Appendix, has been updated and adjusted.

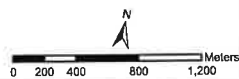


Legend

- - - Keswick Secondary Plan Boundary
- Watercourse
- Waterbody

Data Source

- The Town of Georgina
- The Regional Municipality of York
- LSRCA



PLAN B Natural Heritage

Landscape Ecology & Natural Heritage Planning

© The Planning Board
May 2019

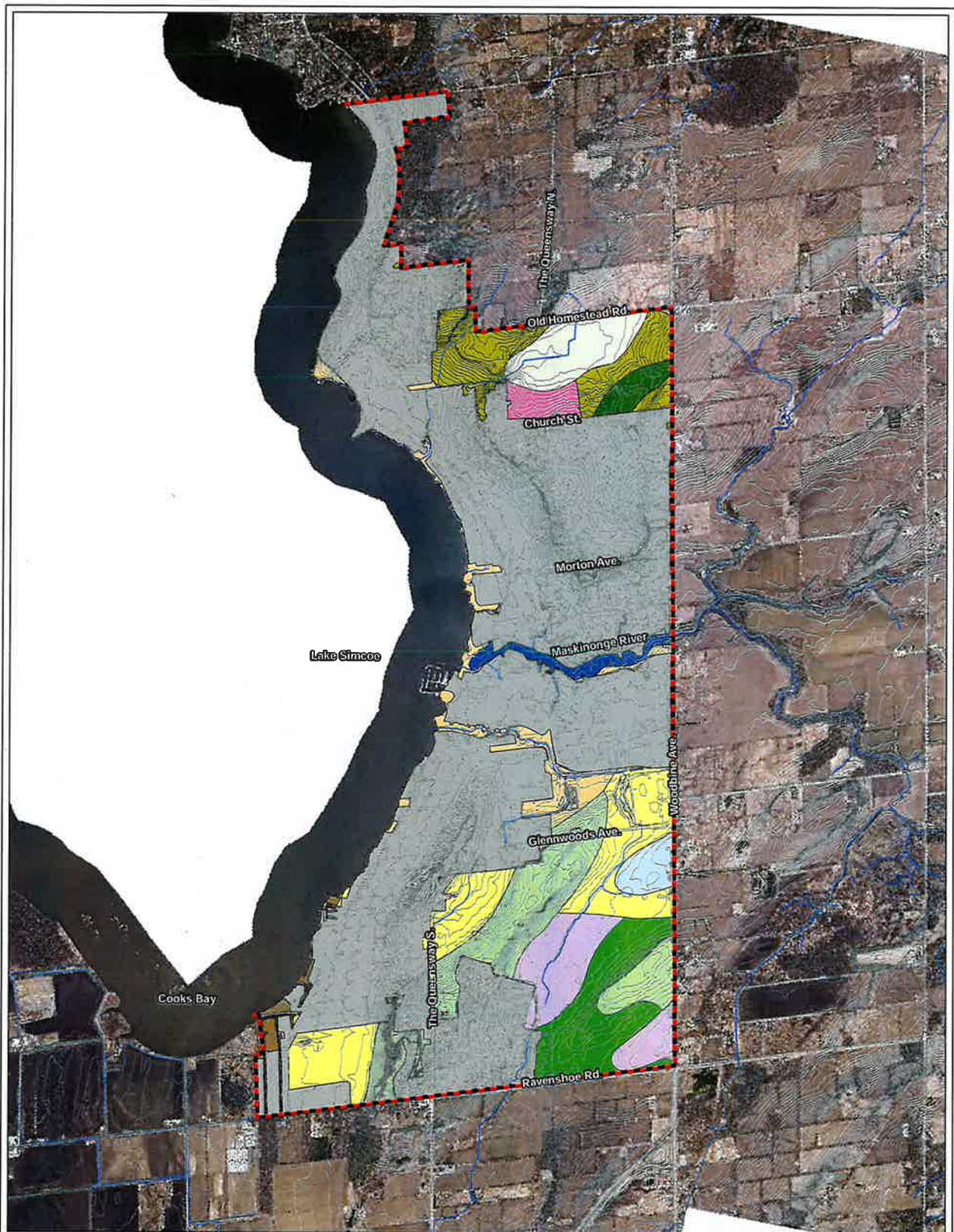
Keswick Secondary Plan Review

Study Area

Project # 2019-178
Date November 2019
Scale 30,000
Prepared By: JJJ | Verified By: BDB

Figure #

1



Legend

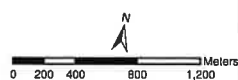
- Keswick Secondary Plan Boundary
- Watercourse
- Contour Line (1 m intervals)

Soil Type

- | | |
|---|---|
| BONDHEAD LOAM | LYONS LOAM |
| BOTTOM LAND | MUCK |
| BUILT UP AREA | OTONABEE SANDY LOAM |
| CASHIEL CLAY | SARGENT SANDY LOAM |
| GRANBY SANDY LOAM | SIMCOE CLAY LOAM |
| GUERIN LOAM | SIMCOE SILT LOAM |
| GUERIN SANDY LOAM | |
| JEDDO CLAY LOAM | |
| KING CLAY LOAM | |

Data Source

- The Town of Georgina
- LSRCA
- © Queen's Printer for Ontario, 2019



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Keswick Secondary Plan Review Soils and Topography

Project # 2019-178

Date November 2019

Scale 30,000

Prepared By: JJ Verified By: BDB

Figure #

2

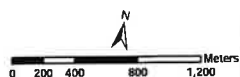


Legend

- Keswick Secondary Plan Boundary
- Watercourse
- Meanderbelt
- Regulation Limit
- Waterbody
- Floodplain
- Wetland
- Lake Shoreline Erosion Limit

Data Source

- The Town of Georgina
- The Regional Municipality of York
- LSRCA



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116 E. Howard Street
Windsor, ON
(519 711 1111)

Keswick Secondary Plan Review

LSRCA Regulated Features

Project # **2019-178**
Date **November 2019**
Scale **30,000**
Prepared By: JJJ | Verified By: BDB

Figure #

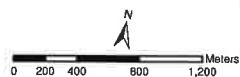
3



Legend

- Keswick Secondary Plan Boundary
- Watercourse
- Waterbody
- Wetland
- Highly Vulnerable Aquifers
- Significant Groundwater Recharge Areas
- Intake Protection Zone 1
- Intake Protection Zone 2
- Ecologically Significant Groundwater Recharge Areas

Data Source
 - The Town of Georgina
 - The Regional Municipality of York
 - LSRCA
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814 Parkway Drive, Unit 10
 Richmond Hill, ON L4B 1N1
 905.709.1000

Keswick Secondary Plan Review Hydrogeological Sensitive Areas

Project # 2019-178
 Date November 2019
 Scale 30,000
 Prepared By: JJJ | Verified By: BOB

Figure #

4



Legend

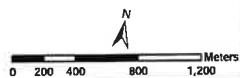
- - - Keswick Secondary Plan Boundary
- Watercourse
- Waterbody

Vegetation Communities

- | | |
|--|--|
| ■ Cultural Meadow (CUM) | ■ Open Aquatic (OAO) |
| ■ Cultural Plantation (CUP) | ■ Mixed Shallow Aquatic (SAM) |
| ■ Cultural Thicket (CUT) | ■ Submerged Shallow Aquatic (SAS) |
| ■ Cultural Woodland (CUW) | ■ Coniferous Swamp (SWC) |
| ■ Coniferous Forest (FOC) | ■ Deciduous Swamp (SWD) |
| ■ Deciduous Forest (FOD) | ■ Mixed Swamp (SWM) |
| ■ Mixed Forest (FOM) | ■ Thicket Swamp (SWT) |
| ■ Meadow Marsh (MAM) | |
| ■ Shallow Marsh (MAS) | |

Data Source

- The Town of Georgina
- The Regional Municipality of York
- LSRCA



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110, FORT PERRY, ONTARIO
WATERLOO, ONTARIO
N2L 2G5

Keswick Secondary Plan Review

Vegetation Communities

Project # 2019-178

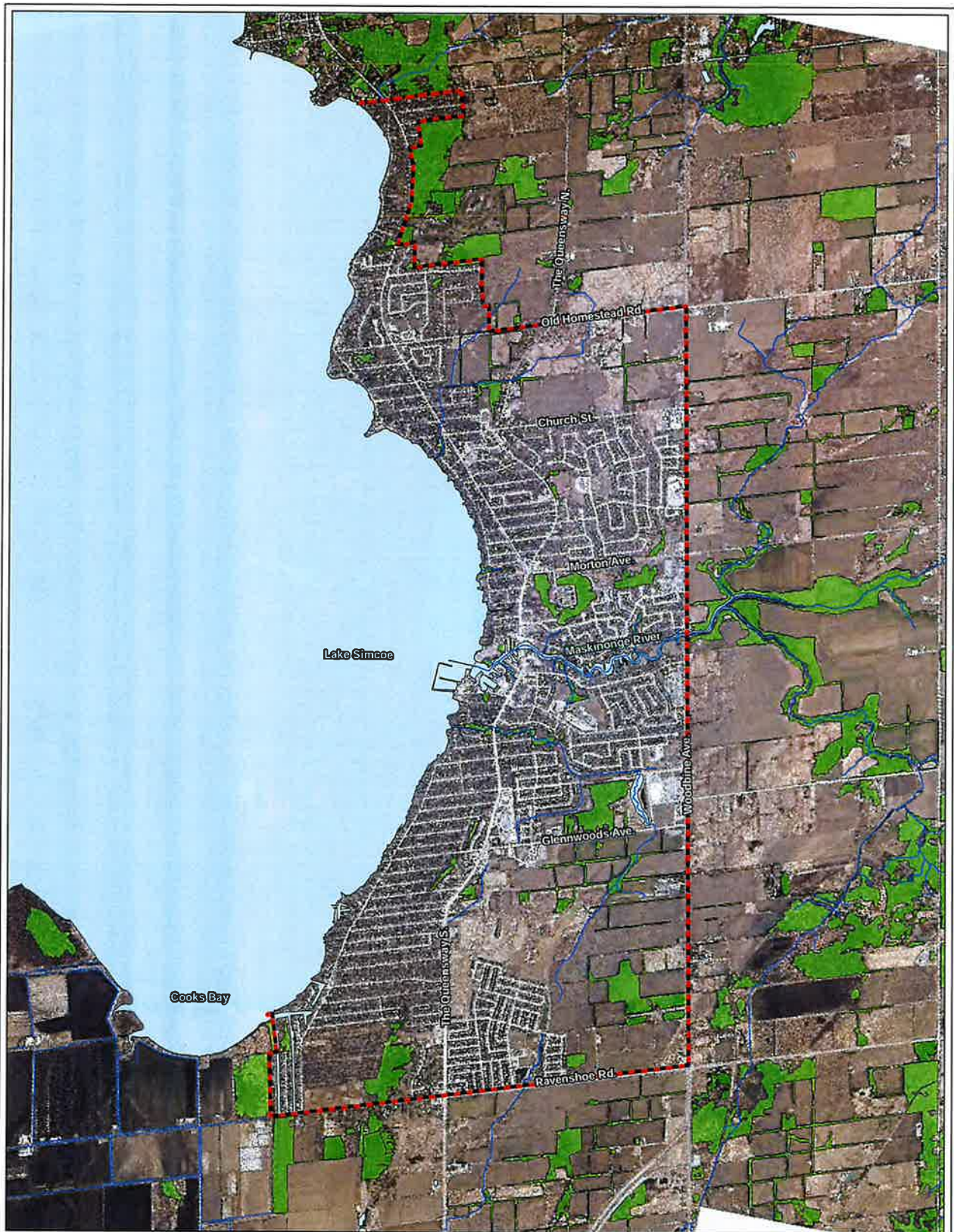
Date November 2019

Scale 30,000

Prepared By: JJJ Verified By: BDB

Figure #

5

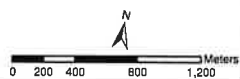


Legend

- Keswick Secondary Plan Boundary
- Watercourse
- Waterbody
- Woodland

Data Source

- The Town of Georgina
- The Regional Municipality of York
- LSROA
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PLAN B Natural Heritage

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Keswick Secondary Plan Review

Woodlands

Project # 2019-178

Date November 2019

Scale 30,000

Prepared By: JJJ | Verified By: BOB

Figure #

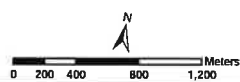
6



Legend

- Keswick Secondary Plan Boundary
- Watercourse
- Waterbody
- Wetland - Evaluated as Provincially Significant
- Wetland - Not Evaluated per the Ontario Wetland Evaluation System

Data Source
 - The Town of Georgina
 - The Regional Municipality of York
 - LSRCA



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Landscape Ecology & Natural Heritage Planning
 116 Filmonk Crescent
 Wainwright, ON
 (548-39-1)

Keswick Secondary Plan Review

WETLANDS

Project # 2019-178

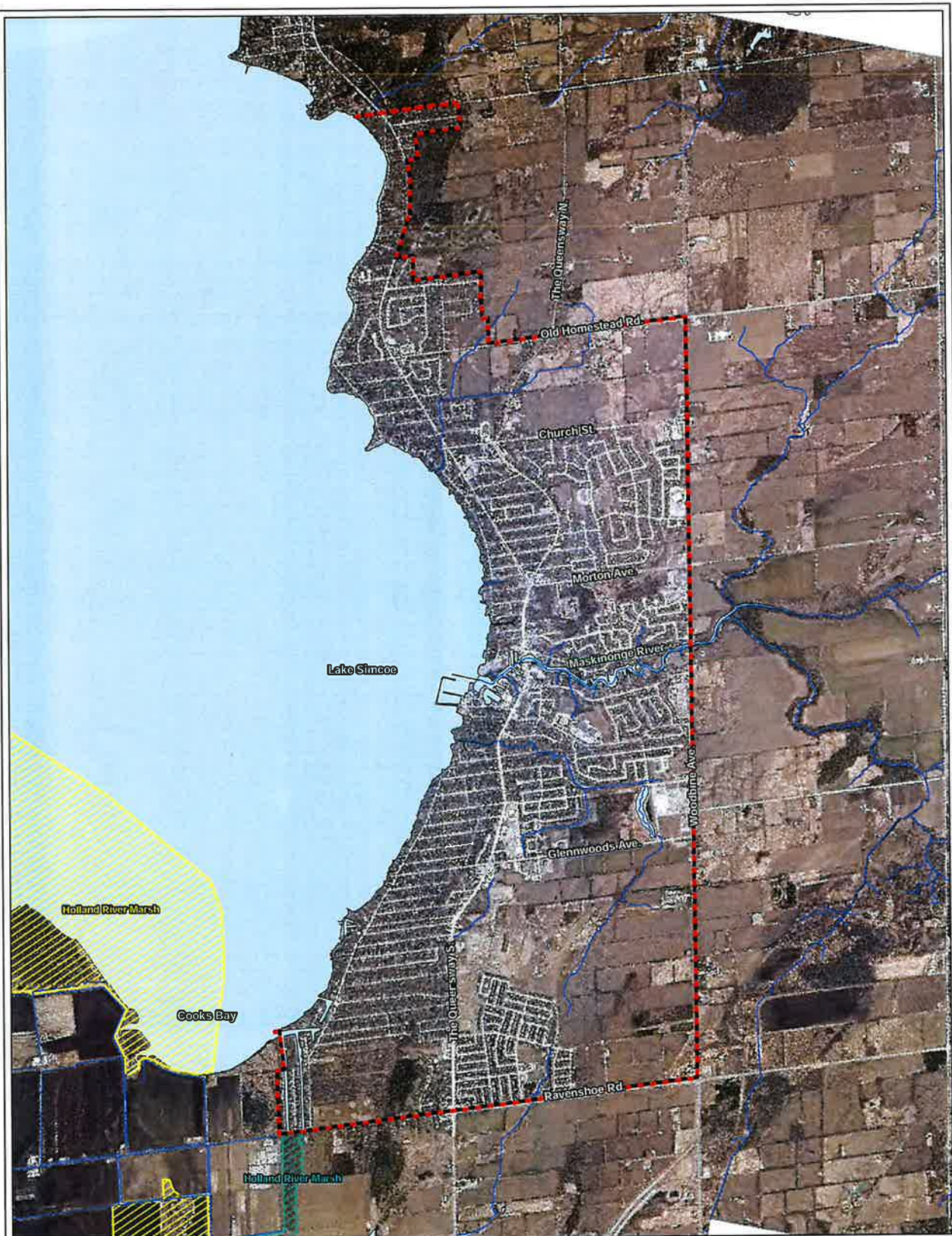
Date November 2019

Scale 30,000

Prepared By: JJJ Verified By: BDB

Figure #

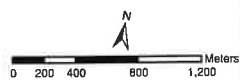
7



Legend

- Keswick Secondary Plan Boundary
- Watercourse
- Waterbody
- Area of Natural and Scientific Interest (Life Science) - Provincial
- Candidate Area of Natural and Scientific Interest (Life Science) - Provincial

Data Source
 - The Town of Georgina
 - LSRCA
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PLAN B Natural Heritage

Landscape Ecology & Natural Heritage Planning

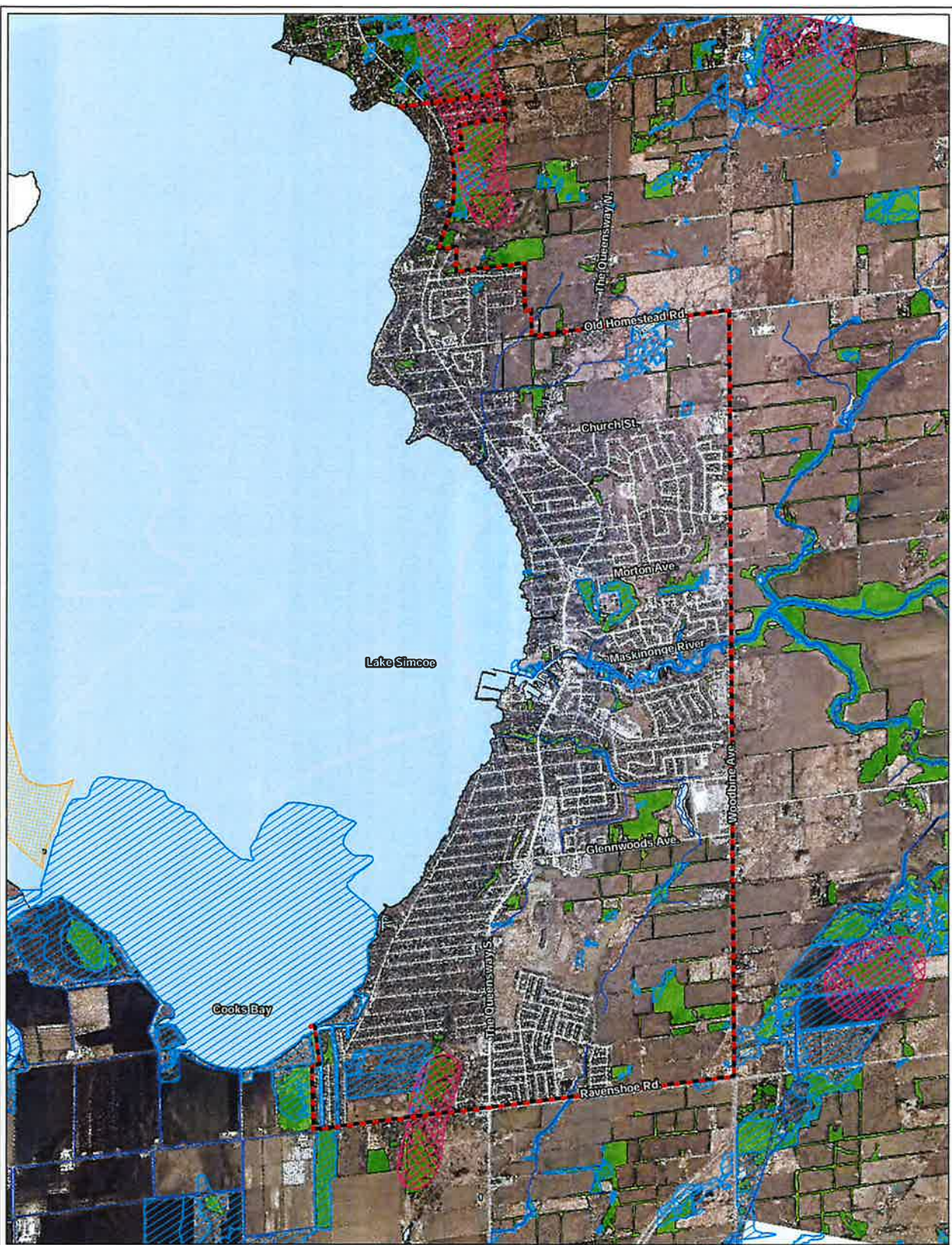
Keswick Secondary Plan Review

AREAS OF NATURAL AND SCIENTIFIC INTEREST

Project # 2019-178
 Date November 2019
 Scale 30,000
 Prepared By: JJJ Verified By: BDB

Figure #

8

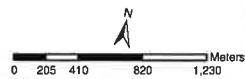


Legend

- Keswick Secondary Plan Boundary
- Watercourse
- Waterbody
- Wetland
- Woodland
- Deer Wintering Area (Stratum 2)
- Waterfowl Staging Area

Data Source

- The Town of Georgina
- The Regional Municipality of York
- LSRCA
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PLAN B Natural Heritage

Landscape Ecology & Natural Heritage Planning

Keswick Secondary Plan Review

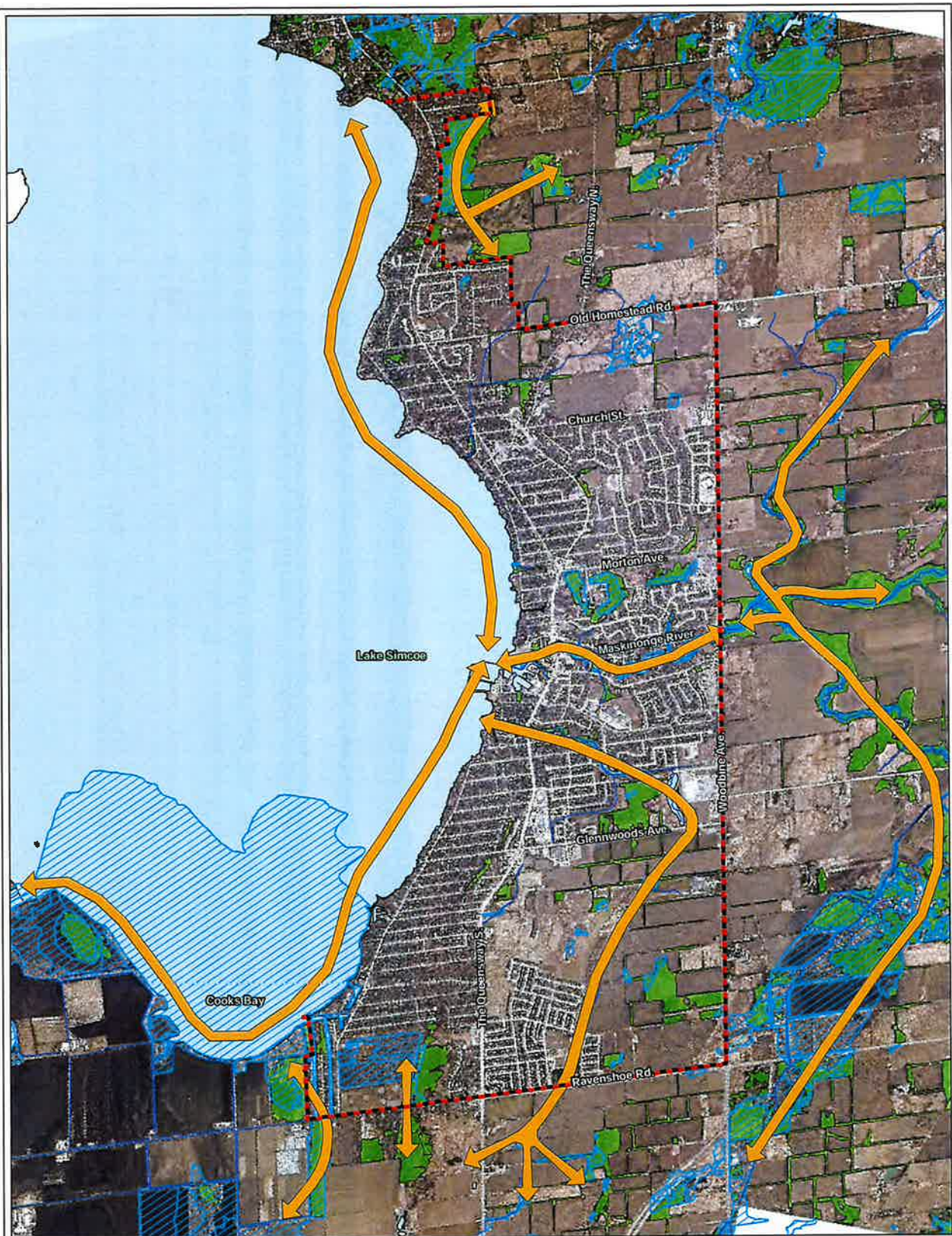
Significant Wildlife Habitat Features

Project # 2019-178
Date November 2019
Scale 30,000

Figure #

9

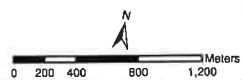
Prepared By: JJJ Verified By: BDB



Legend

- Keswick Secondary Plan Boundary
- Watercourse
- Waterbody
- Wetland
- Woodland
- ➔ Species Dispersal Corridors

Data Source
 - The Town of Georgina
 - The Regional Municipality of York
 - LSRCA
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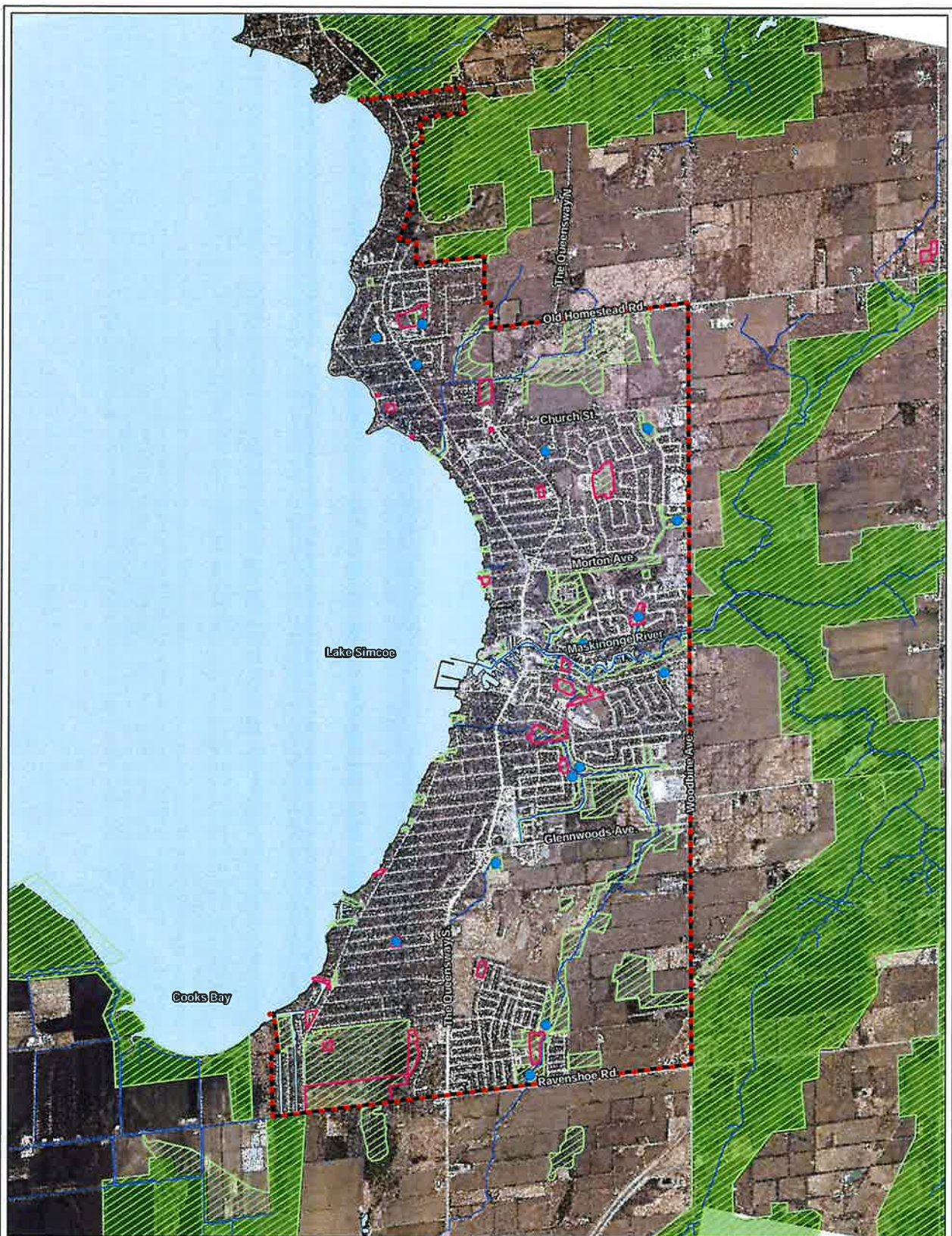
PLAN B Natural Heritage

Landscape Ecology & Natural Heritage Planning

Keswick Secondary Plan Review

Species Dispersal Corridors and Connections

Project #	2019-178	Figure #
Date	November 2019	10
Scale	30,000	
Prepared By: JJJ	Verified By: BDB	

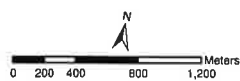


Legend

- Keswick Secondary Plan Boundary
- Watercourse
- Town Park
- Storm Water Management Pond
- Waterbody
- Greenbelt Plan NHS
- Greenlands System (Region of York/Town of Georgina)

Data Source

- The Town of Georgina
- The Regional Municipality of York
- LSRCA
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PLAN B Natural Heritage

Landscape Ecology & Natural Heritage Planning

Keswick Secondary Plan Review Preliminary NHS

Project # 2019-178

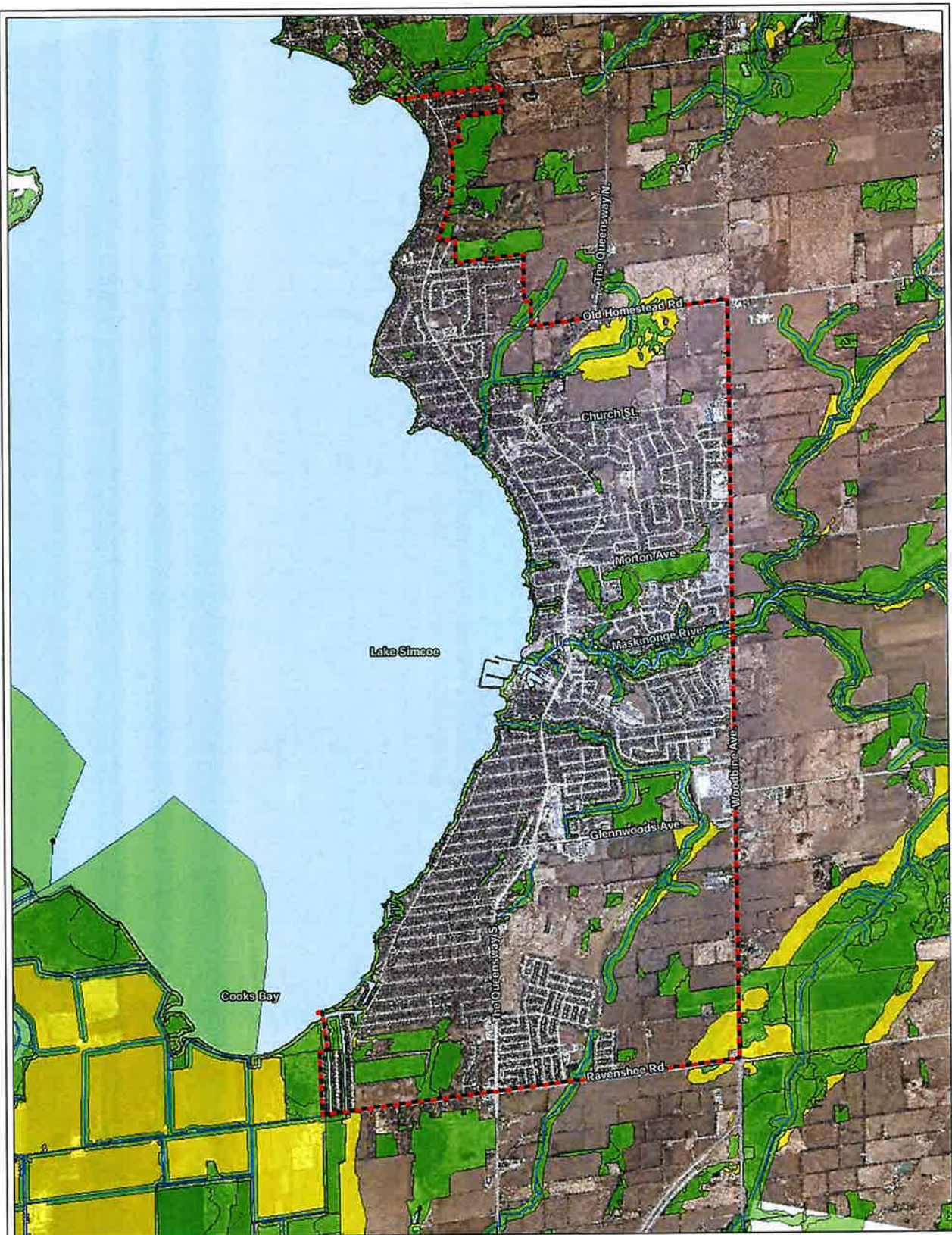
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Scale 30,000

Prepared By: JJJ Verified By: BDB

Figure #

11

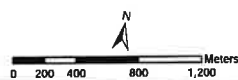


Legend

- Keswick Secondary Plan Boundary
- Watercourse
- Waterbody
- Core Natural Heritage Feature
- Corridor/Floodplain Enhancement Opportunity

Data Source

- The Town of Georgina
- The Regional Municipality of York
- LSRCA



PLAN B Natural Heritage

Landscape Ecology & Natural Heritage Planning

111 Fiddlers' Creek
Walden, ON
L0K 2H2

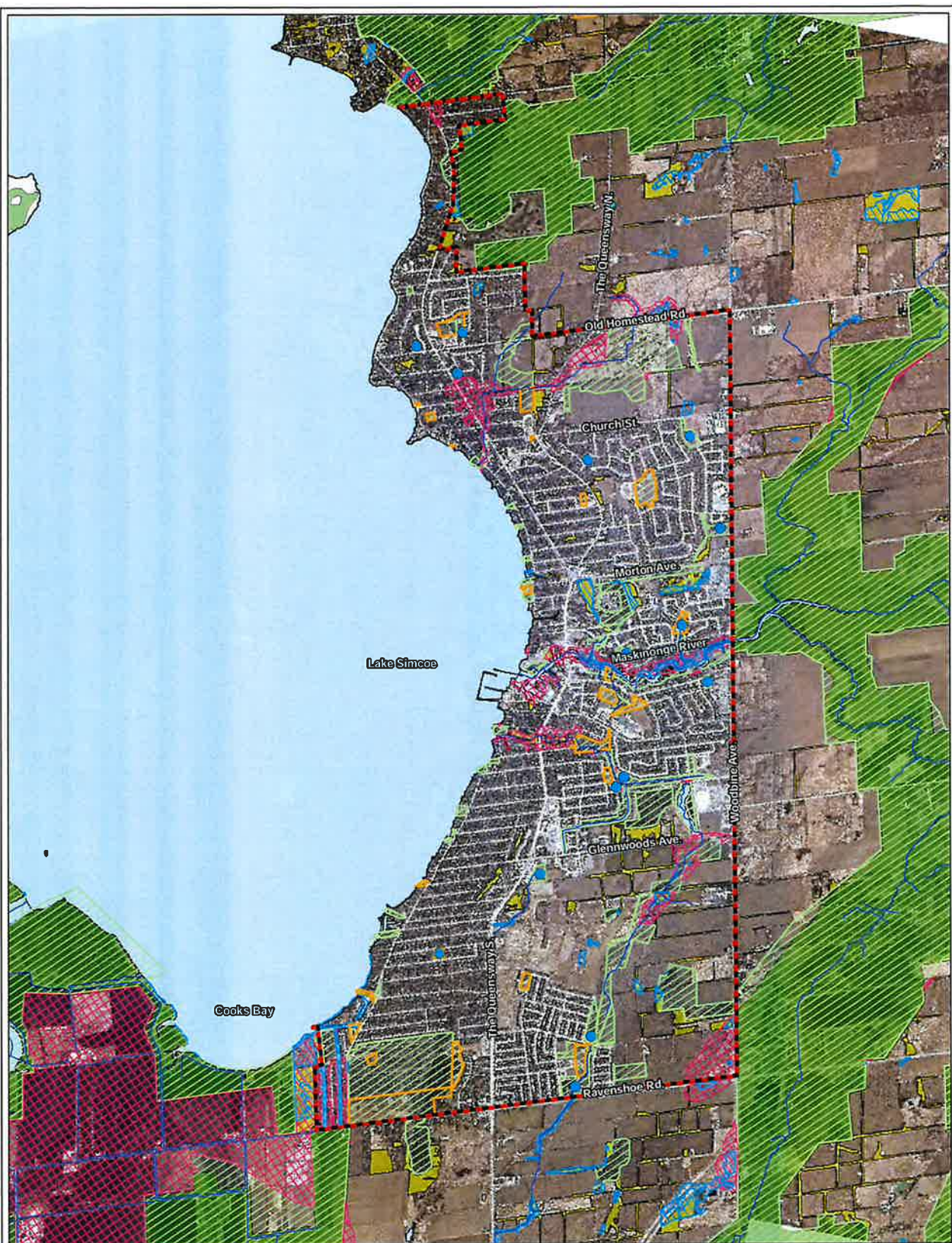
Keswick Secondary Plan Review

Core Natural Areas and Floodplain/Corridor Enhancements (LSRCA)

Project # 2019-178
Date November 2019
Scale 30,000
Prepared By: JJJ Verified By: BDB

Figure #

12



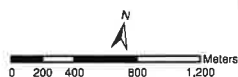
Legend

- Keswick Secondary Plan Boundary
- Watercourse
- Town Park
- Storm Water Management Pond
- Waterbody
- Wetland
- Woodland
- Floodplain

Data Source

- The Town of Georgina
- The Regional Municipality of York
- LSROA
- © Queen's Printer for Ontario, 2019

- Greenbelt Plan NHS
- Greenlands System (Region of York/Town of Georgina)



PLAN B Natural Heritage

Landscape Ecology & Natural Heritage Planning

100 Main Street
Georgina, ON
L7G 3K7

Keswick Secondary Plan Review NHS Features Outside of the Town/Region Greenlands System

Project # 2019-178

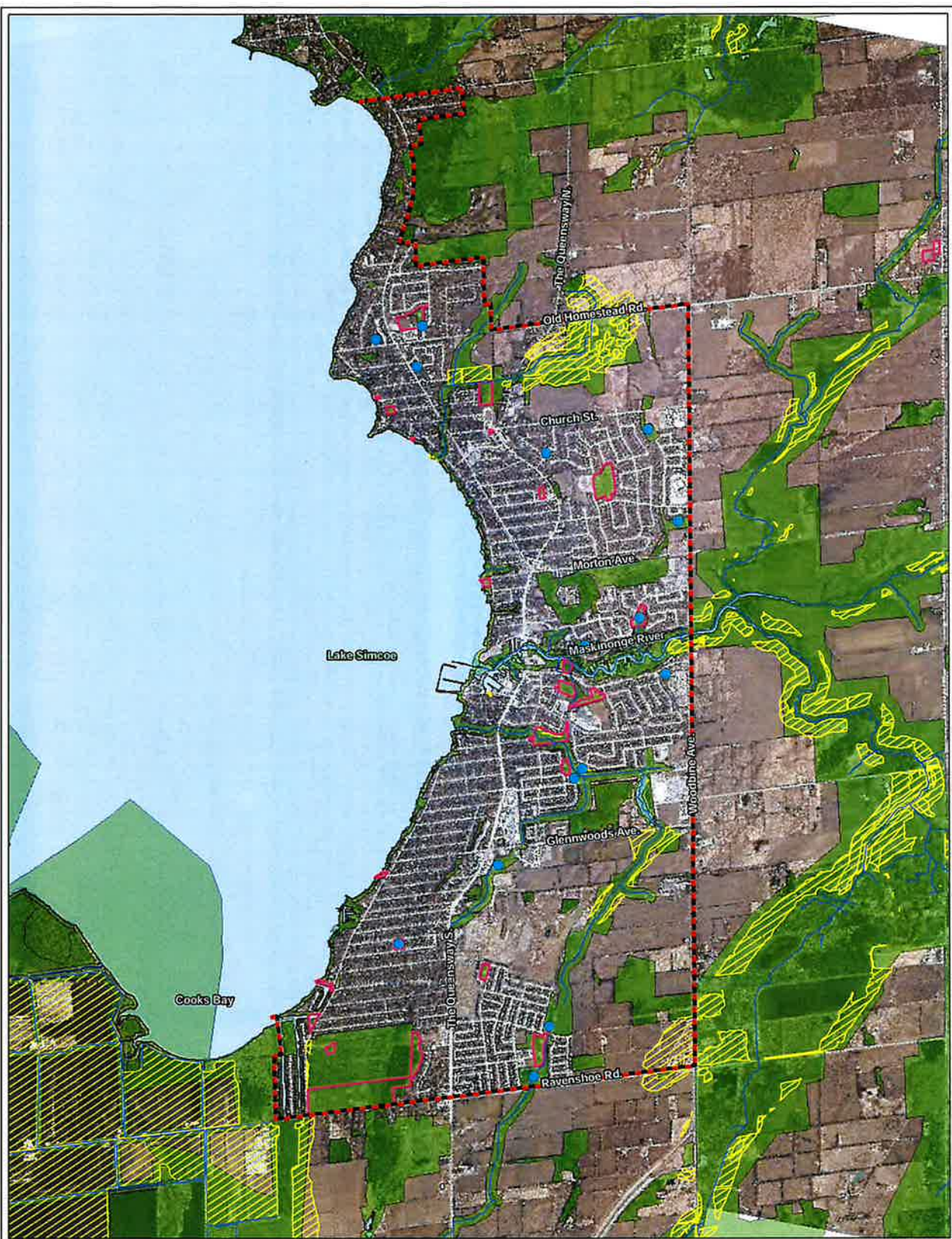
Date November 2019

Scale 30,000

Prepared By: JJJ Verified By: BOB

Figure #

13

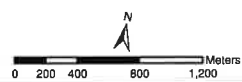


Legend

- - - Keswick Secondary Plan Boundary
- Watercourse
- Storm Water Management Pond
- Waterbody
- Natural Heritage System
- - - Enhancement Opportunities

Data Source

- The Town of Georgina
- The Regional Municipality of York
- LSRCA
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PLAN B Natural Heritage

Landscape Ecology & Natural Heritage Planning

Keswick Secondary Plan Review Composite Natural Heritage System (Town/Region/LSRCA)

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Date November 2019

Scale 30,000

Prepared By: JJJ Verified By: HOB

Figure #

14